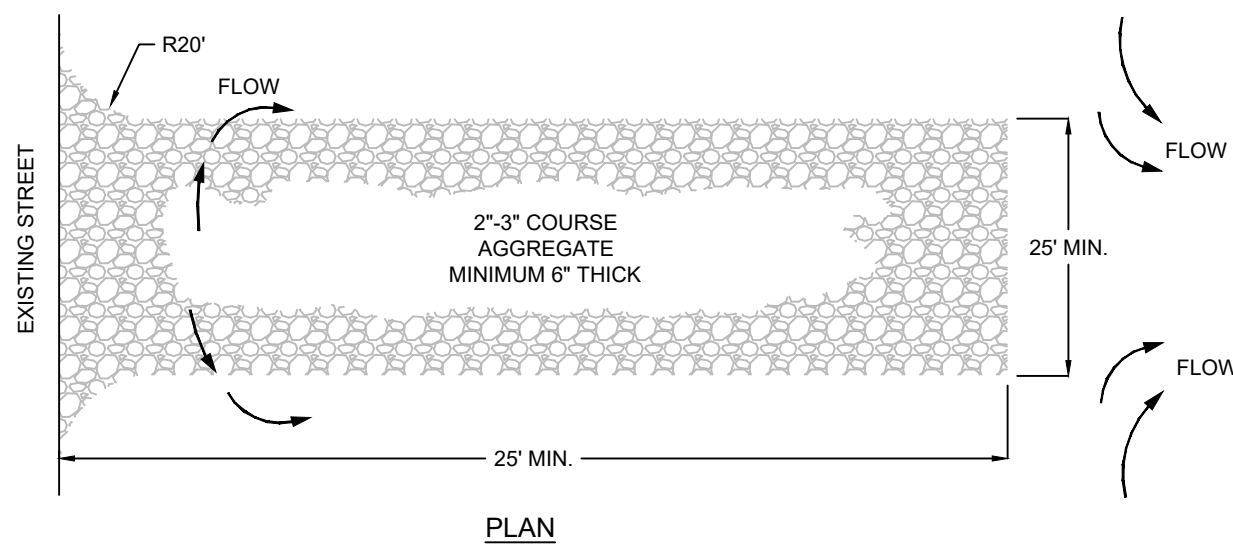


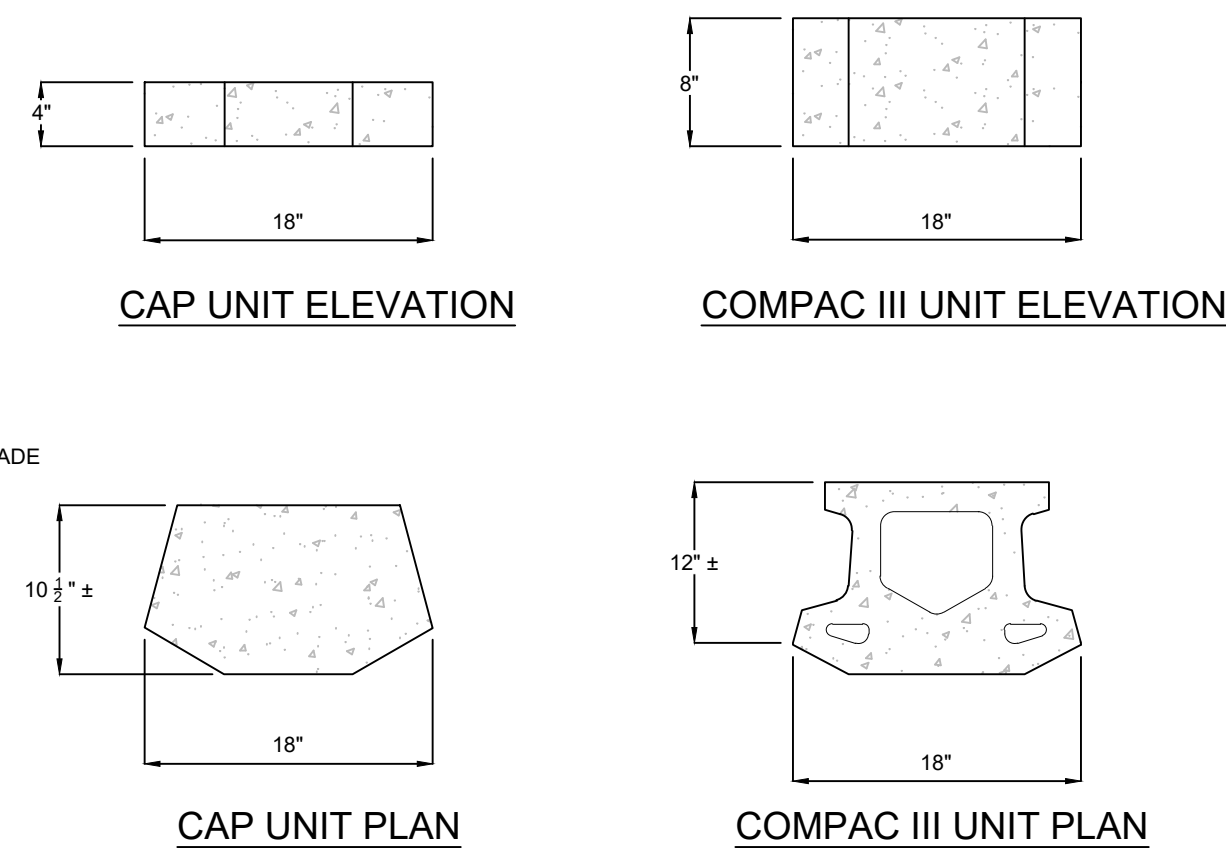
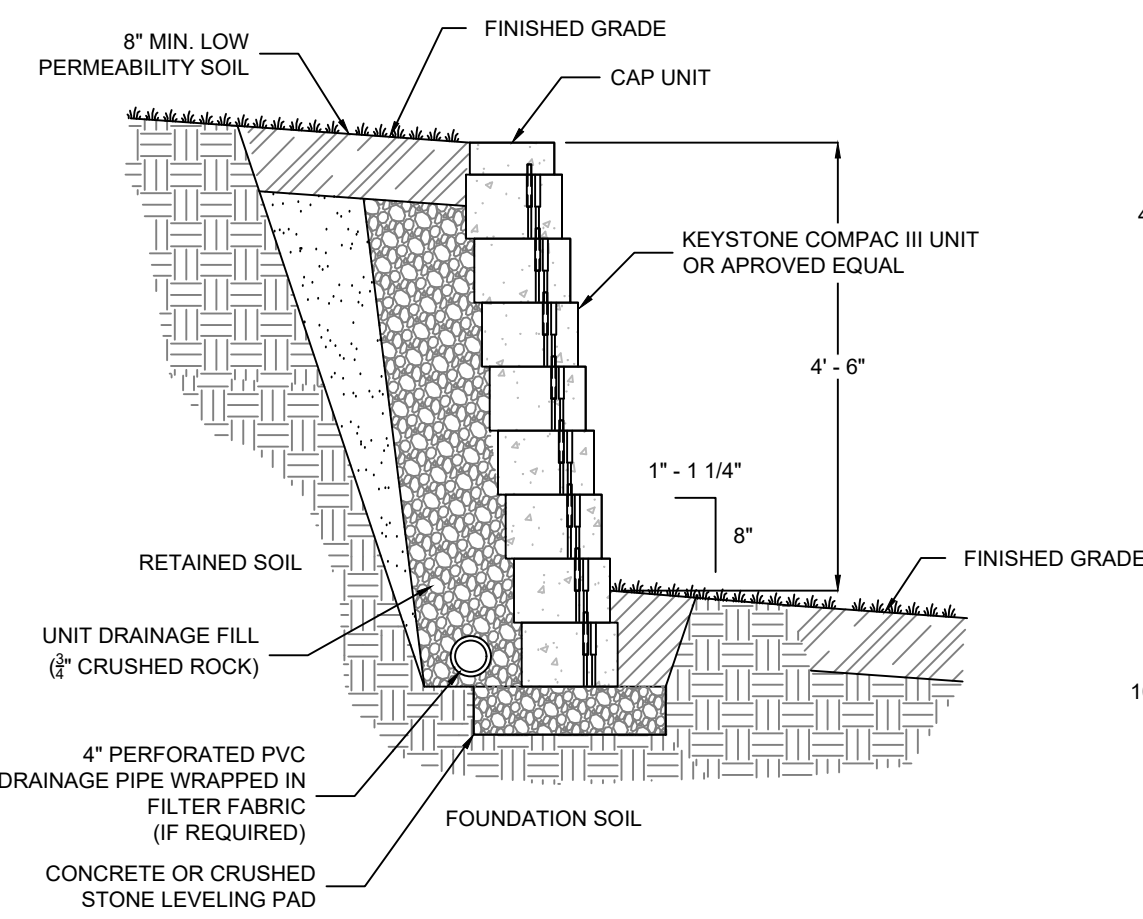
- NOTES:**
1. ALL MATERIAL TO MEET MANUFACTURER'S REQUIREMENTS.
  2. FILTER SOCK DEPICTED IS FOR MINIMUM SLOPES. GREATER SLOPES MAY REQUIRE LARGER SOCKS PER ENGINEER.
  3. COMPOST MATERIAL TO BE DISPERSED ON SITE, AS DETERMINED BY ENGINEER.

**COMPOST FILTER SOCK DETAIL**  
SCALE: NONE



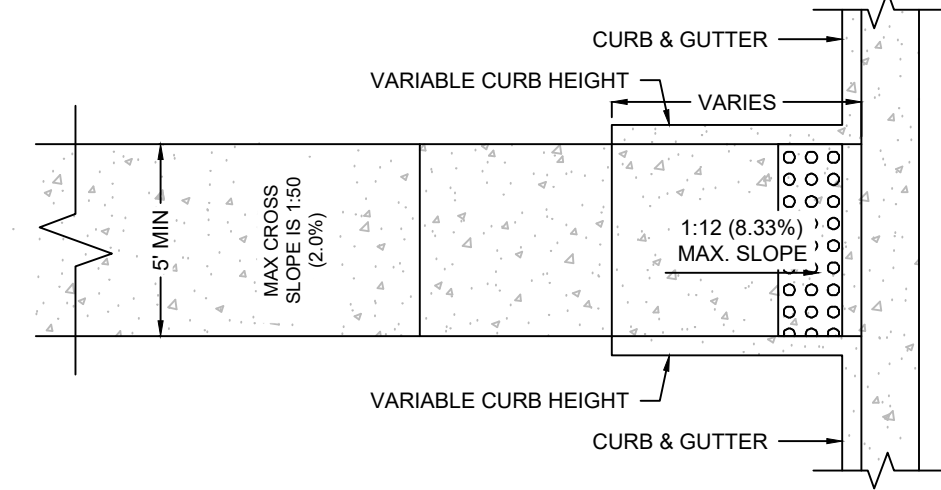
- NOTES:**
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT.
  2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
  3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.

**TEMPORARY CONSTRUCTION ENTRANCE**  
SCALE: NONE



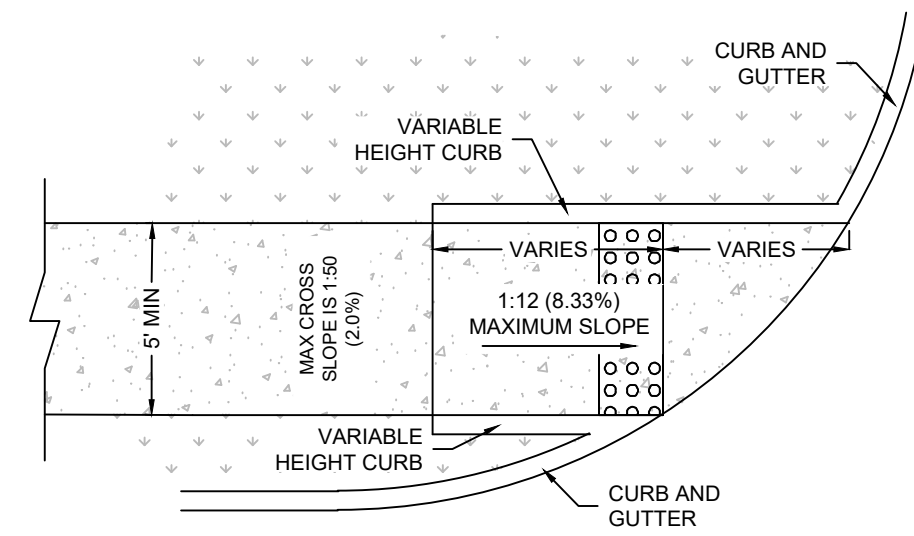
- NOTES:**
1. ONE DEGREE OR ZERO DEGREE BATTER ANGLE WALLS ARE AVAILABLE USING BLOCKS WITH 7 1/2" (190 MM) OR 6 3/4" (171 MM) KNOBS (SPECIALTY ITEMS).
  2. BLOCK MANUFACTURER TO PROVIDE ENGINEERING DESIGN, SEALED BY A PROFESSIONAL ENGINEER, LICENSED IN THE STATE OF MISSOURI.
  3. BLOCK MANUFACTURER TO PROVIDE CALCULATIONS THAT ENSURE THE EXTENT OF OVER-EXCAVATION AND GEGRID REINFORCEMENT LENGTHS INTO EMBANKMENT PROVIDE ADEQUATE RESISTANCE FROM SOIL LATERAL LOADING, TO THE DEPTHS SHOWN ON THE PLANS, SIGNED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF MISSOURI. REFERENCE THE GEOTECHNICAL ENGINEERING REPORT FOR LOADING CRITERIA.

**BLOCK RETAINING WALL DETAIL**  
SCALE: NONE

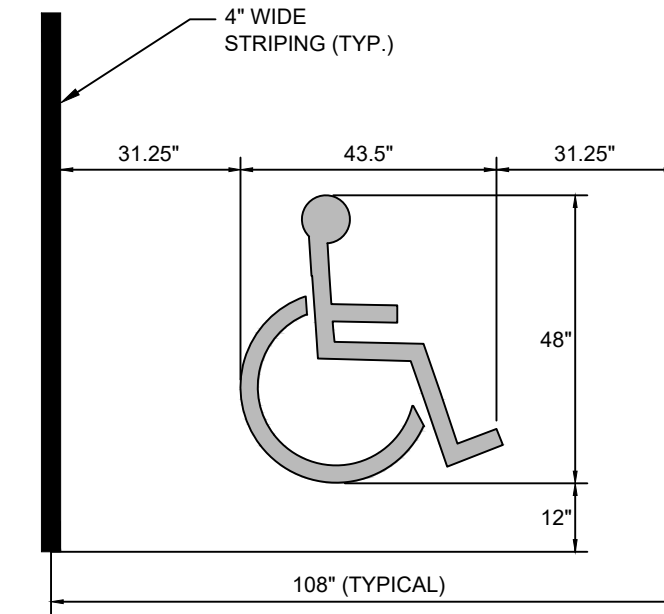


**TYPE 4 CURB RAMP DETAIL**  
SCALE: NONE

- NOTES:**
1. ENSURE THAT THE INSIDE EDGE OF CURVED RAMPS MAINTAIN AN 8.33% SLOPE MAXIMUM.
  2. THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE FOOT OF CURB RAMP RUNS, BLENDED TRANSITIONS AND TURNING SPACES SHALL BE 5% MAXIMUM.
  3. ON PARALLEL CURB RAMPS, DETECTABLE WARNING SURFACES SHALL BE PLACED AS FOLLOWS:
    - A. WHERE THE ENDS OF THE BOTTOM GRADE BREAK ARE IN FRONT OF THE BACK OF CURB, DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BACK OF CURB.
    - B. WHERE THE ENDS OF THE BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS 5.0 FEET OR LESS, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE RAMP RUN WITHIN ONE DOME SPACING OF THE BOTTOM GRADE BREAK.
    - C. WHERE THE ENDS OF THE BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS MORE THAN 5.0 FEET, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE LOWER LANDING AT THE BACK OF CURB.
  4. TRUNCATED DOMES SHALL HAVE A DIAMETER OF 0.9 INCH AT THE BOTTOM, A DIAMETER OF 0.4 INCH AT THE TOP, A HEIGHT OF 0.2 INCH, AND A CENTER-TO-CENTER SPACING OF 1.7 INCHES MEASURED ALONG ONE SIDE OF A SQUARE ARRANGEMENT.

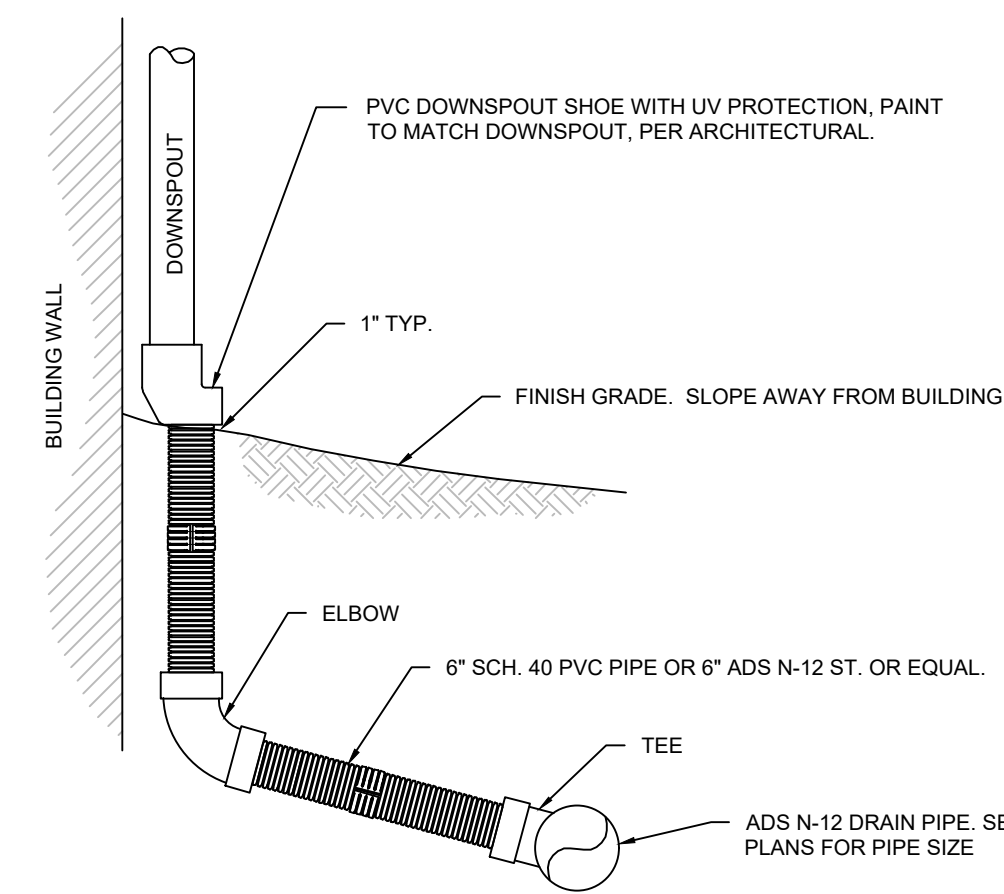


**TYPE 3 CURB RAMP DETAIL**  
SCALE: NONE

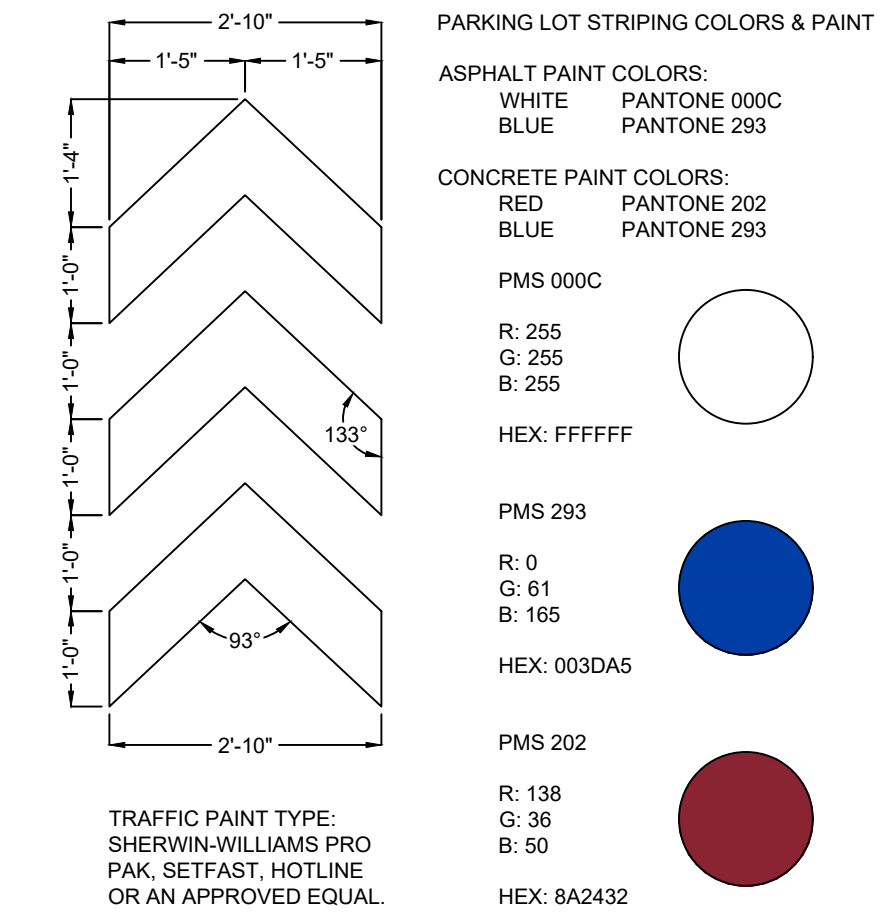


**ADA ACCESSIBLE PARKING SYMBOL**  
SCALE: NONE

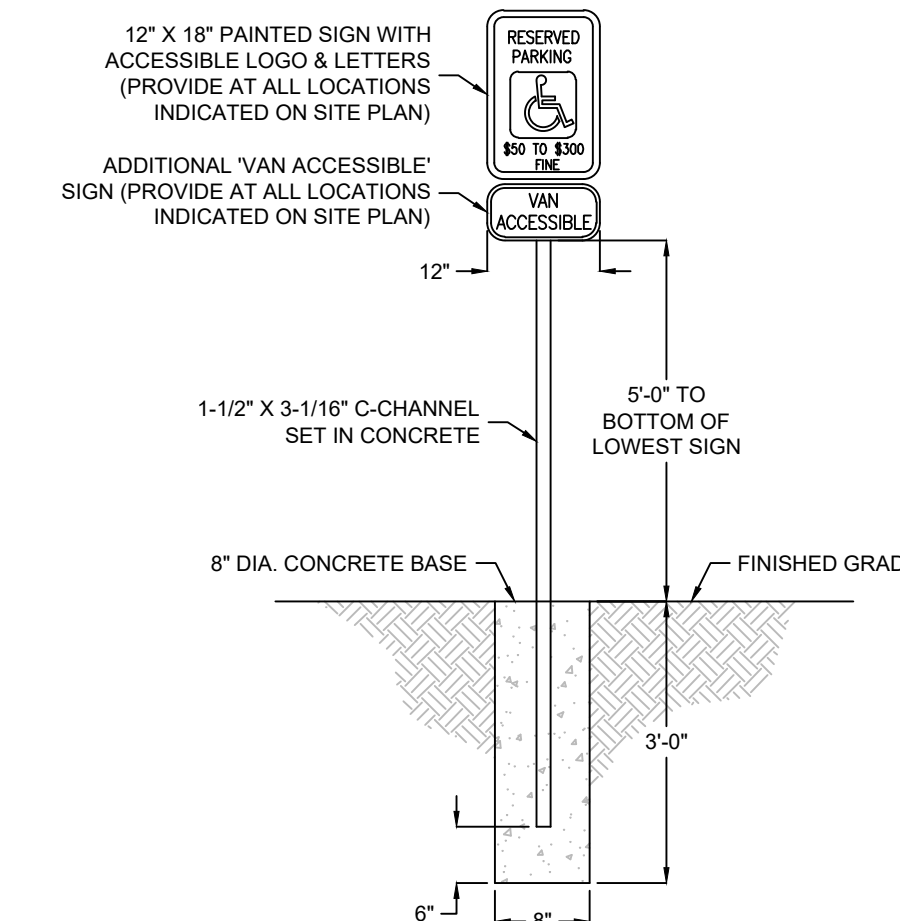
- NOTES:**
1. BOTTOM TO BE POURED IN PLACE.
  2. PIPE TO BE ON GRADE BEFORE BOTTOM IS CONSTRUCTED.
  3. FOR 6" TOP USE 4-#4 DOWELS. ONE IN EACH CORNER W/RAM NEK OR EQUAL.
  4. RAM NEK ALL JOINTS (OR EQUAL).
  5. 6" INVERT REQUIRED TO PREVENT SEDIMENTATION.
  6. THERE MUST BE A 6" MINIMUM CONCRETE SEGMENT ABOVE PRECAST OPENING.
  7. TYPE 'C' RING & COVER.



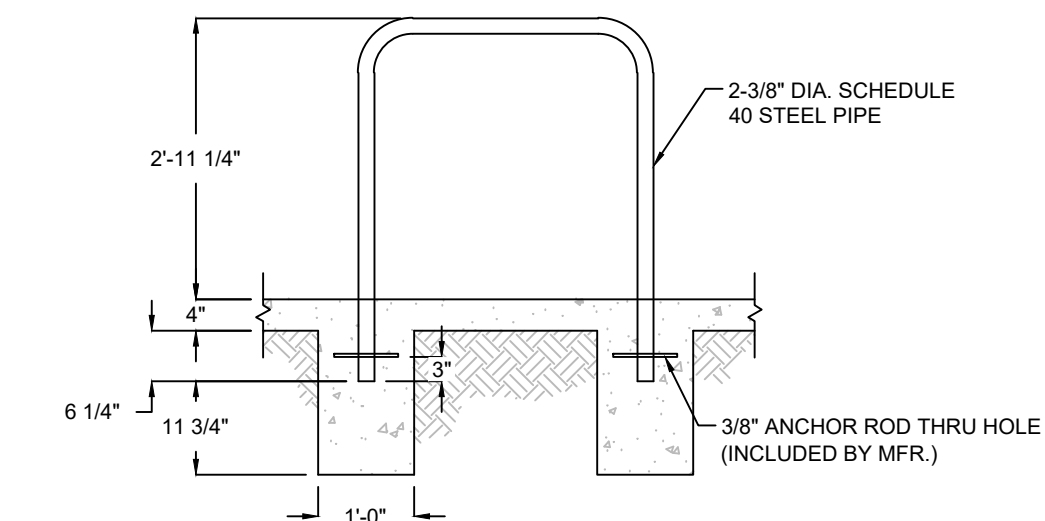
**DOWNSPOUT CONNECTION**  
SCALE: NONE



**PARKING LOT STRIPING COLORS & PAINT**  
SCALE: NONE



**ADA ACCESSIBLE SIGN DETAIL**  
SCALE: NONE



**BICYCLE RACK**  
SCALE: NONE

NOTE: BICYCLE RACK AS APPROVED BY THE ASSOCIATION OF PEDESTRIAN AND BICYCLE PROFESSIONALS.

**TORGERSON DESIGN PARTNERS**  
ARCHITECTURE / REAL ESTATE / DEVELOPMENT

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ARCHITECTURAL CORPORATION MISSOURI LICENSE NUMBER: A-2010011427

**CMC**  
CREATIVE MODULAR CONSTRUCTION

7 BREW COFFEE  
O'FALLON, MO  
1620 STATE HWY K  
O'FALLON, MO 63366

ENGINEER OF RECORD:  
NAME: SHAWN W. BARRY  
LICENSE NO. MO PE-2009001053

PROJECT NUMBER:  
220897BOF

REVISION:  
09/29/2022 CITY REVIEW COMMENTS  
10/19/2022 CITY REVIEW COMMENTS

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**C-702**  
DETAILS  
DATE: JULY 22, 2022