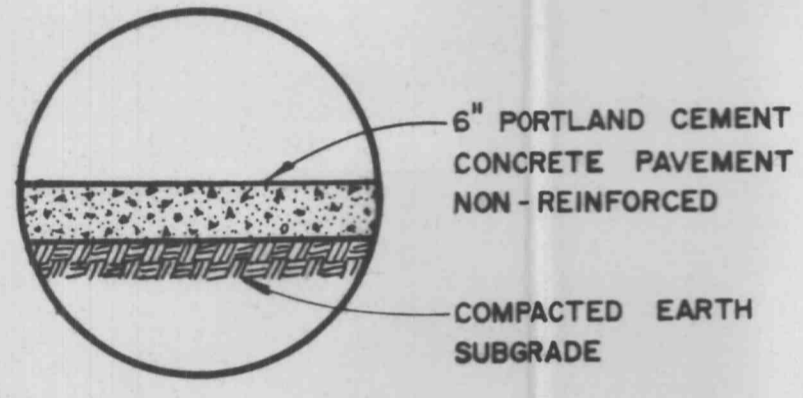
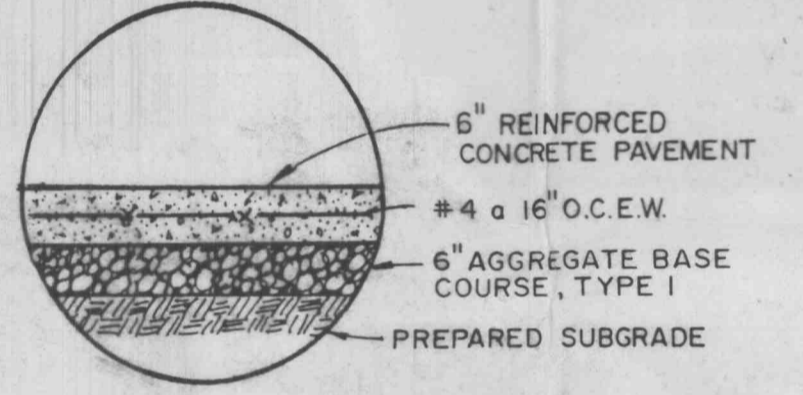


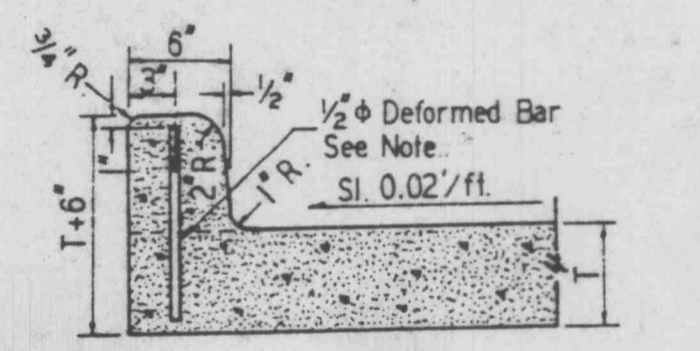
1 CONCRETE PAVEMENT FOR DRIVES



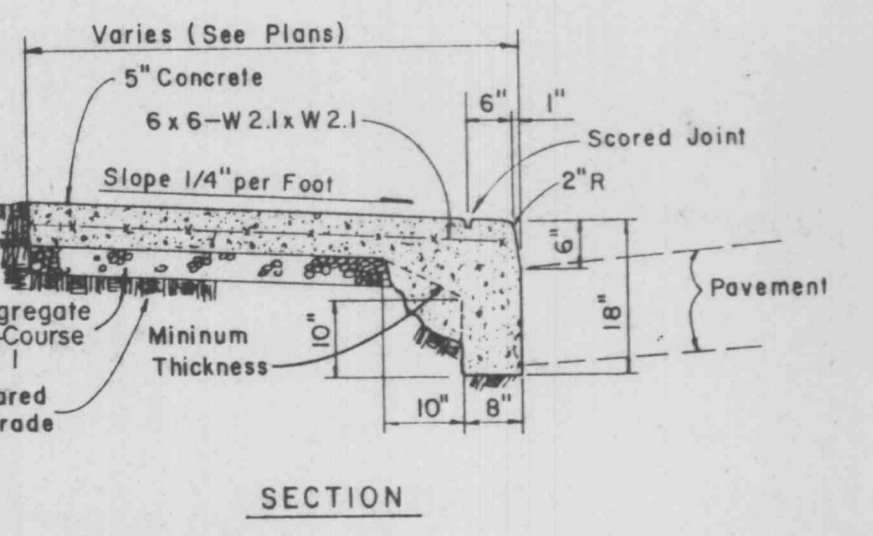
2 CONCRETE PAVEMENT FOR PARKING LOTS



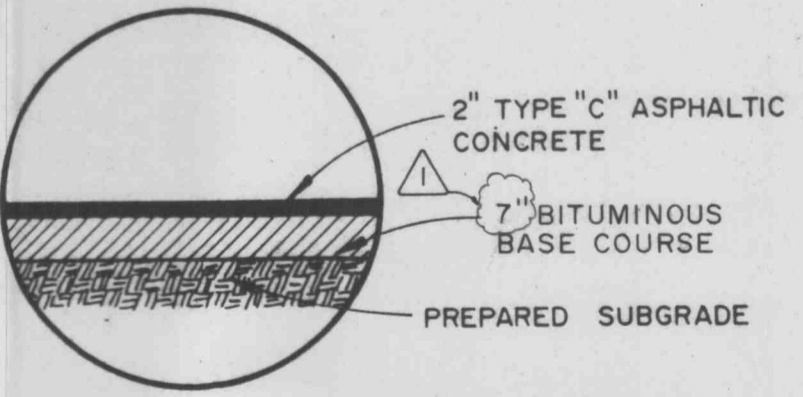
3 CONCRETE PAVEMENT FOR TRUCK AREAS



4 INTEGRAL VERTICAL CURB



5 CONCRETE BARRIER CURB (OPTIONAL)

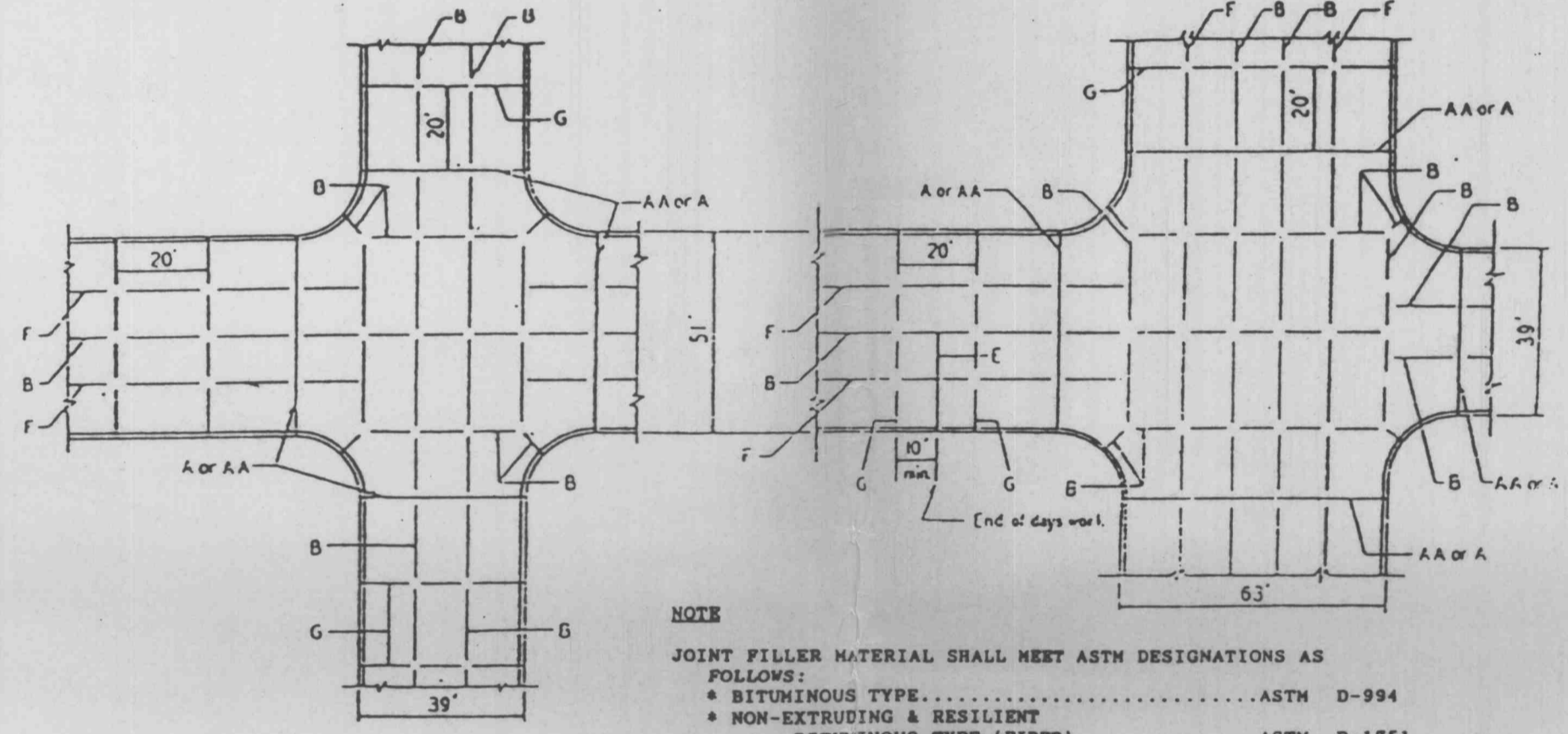


6 COMBINATION SIDEWALK & BARRIER CURB



7 ASPHALT PAVEMENT FOR DRIVES

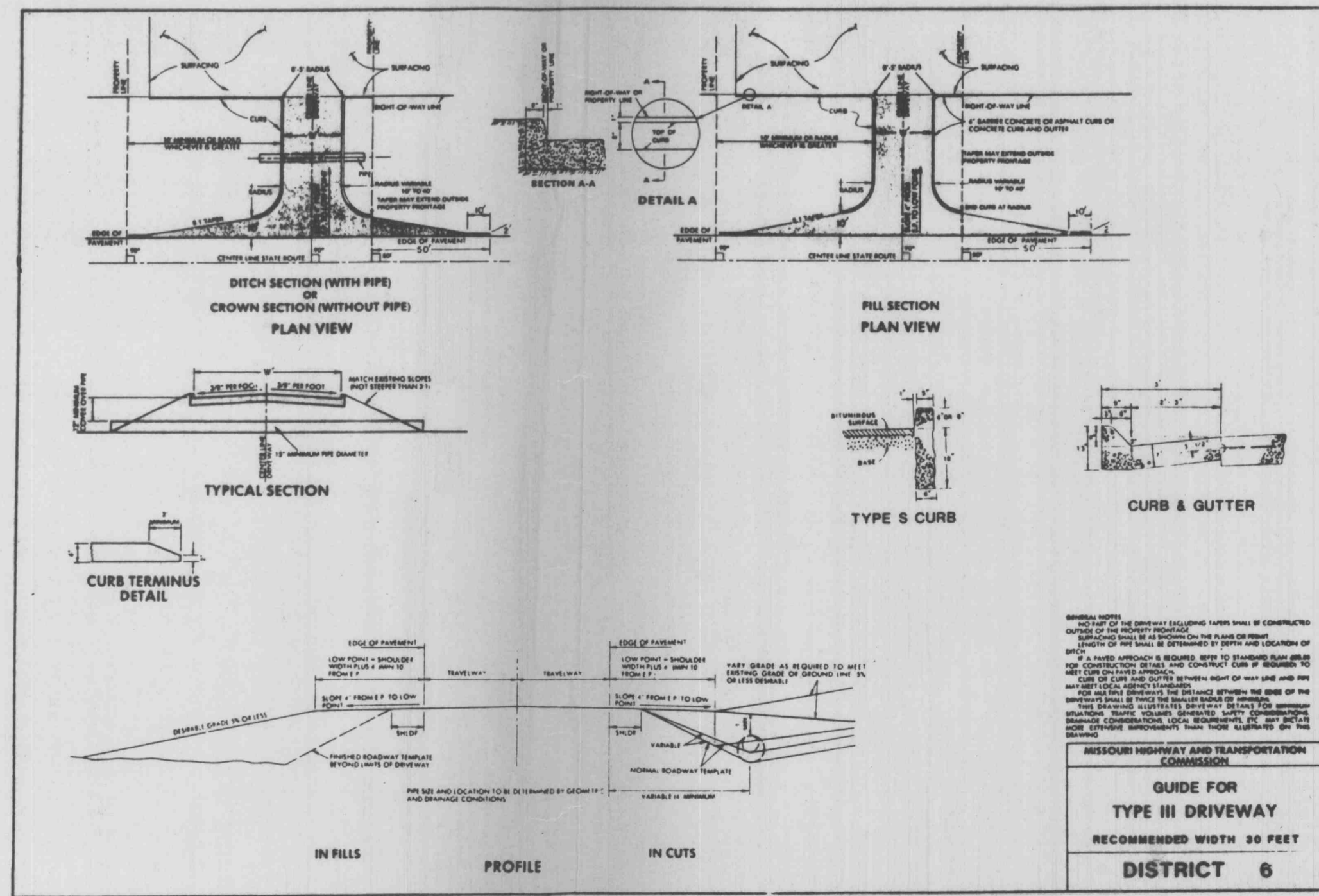
- NOTES**
- DO NOT SCALE DRAWING. FOLLOW DIMENSIONS.
 - ALL STREET INLETS SHALL BE SEPARATED FROM THE PAVEMENT AND CURB BY EXPANSION JOINT MATERIAL EXTENDING COMPLETELY THROUGH CURB AND SLAB. MANHOLE CASTING WITHIN THE PAVEMENT LIMITS SHALL BE BOXED AS SHOWN IN THE "SEWER CONSTRUCTION DETAILS."
 - WHEN A JOINT FALLS WITHIN 5 FT. OF OR CONTACTS INLETS, MANHOLES, OR OTHER STRUCTURES, SHORTEN ONE OR MORE PANELS EITHER SIDE OF OPENING TO PERMIT JOINT TO FALL ON ROUND STRUCTURES AND AT OR BETWEEN CORNERS OF RECTANGULAR STRUCTURES.
 - MINIMUM THICKNESS OF CONCRETE PAVEMENT IS 6 INCHES.
 - CONSTRUCTION JOINT AND TIE BARS MAY BE OMITTED WHEN CURB IS POURED INTEGRAL WITH PAVEMENT.
 - TYPE F JOINT FOR FULL WIDTH CONSTRUCTION AND TYPE B JOINT FOR HALF WIDTH CONSTRUCTION.
 - REFER TO PAVEMENT CONSTRUCTION DETAILS "JOINT AND CURBS" FOR JOINT AND BAR REQUIREMENTS. NOTE THAT WIDTH AND LOCATION OF EACH POURED PORTION OF THE PAVEMENT MAY CHANGE THE TYPE AND LOCATION OF JOINT REQUIRED.
 - TRANSVERSE OR LONGITUDINAL CONSTRUCTION JOINTS IN SLIP FORMED PAVEMENTS MAY BE MADE WITH A GROOVER OR TOOL, IF SUCH DEVICE HAS BEEN APPROVED IN ADVANCE.
 - THE LOCATIONS OF THE TYPE B AND TYPE F LONGITUDINAL CONSTRUCTION JOINTS IN THE SECTIONS MAY BE INTERCHANGED FOR THE DIFFERENT WIDTHS OF CONSTRUCTION IF APPROVED.
 - LONGITUDINAL CONSTRUCTION JOINTS SHALL BE AT A MAXIMUM INTERVAL DISTANCE OF TWENTY (20) FEET AND TRANSVERSE CONSTRUCTION JOINTS SHALL BE AT A MAXIMUM INTERVAL DISTANCE OF FIFTEEN (15) FEET WITHIN TRUCK APRON PARKING AREAS.



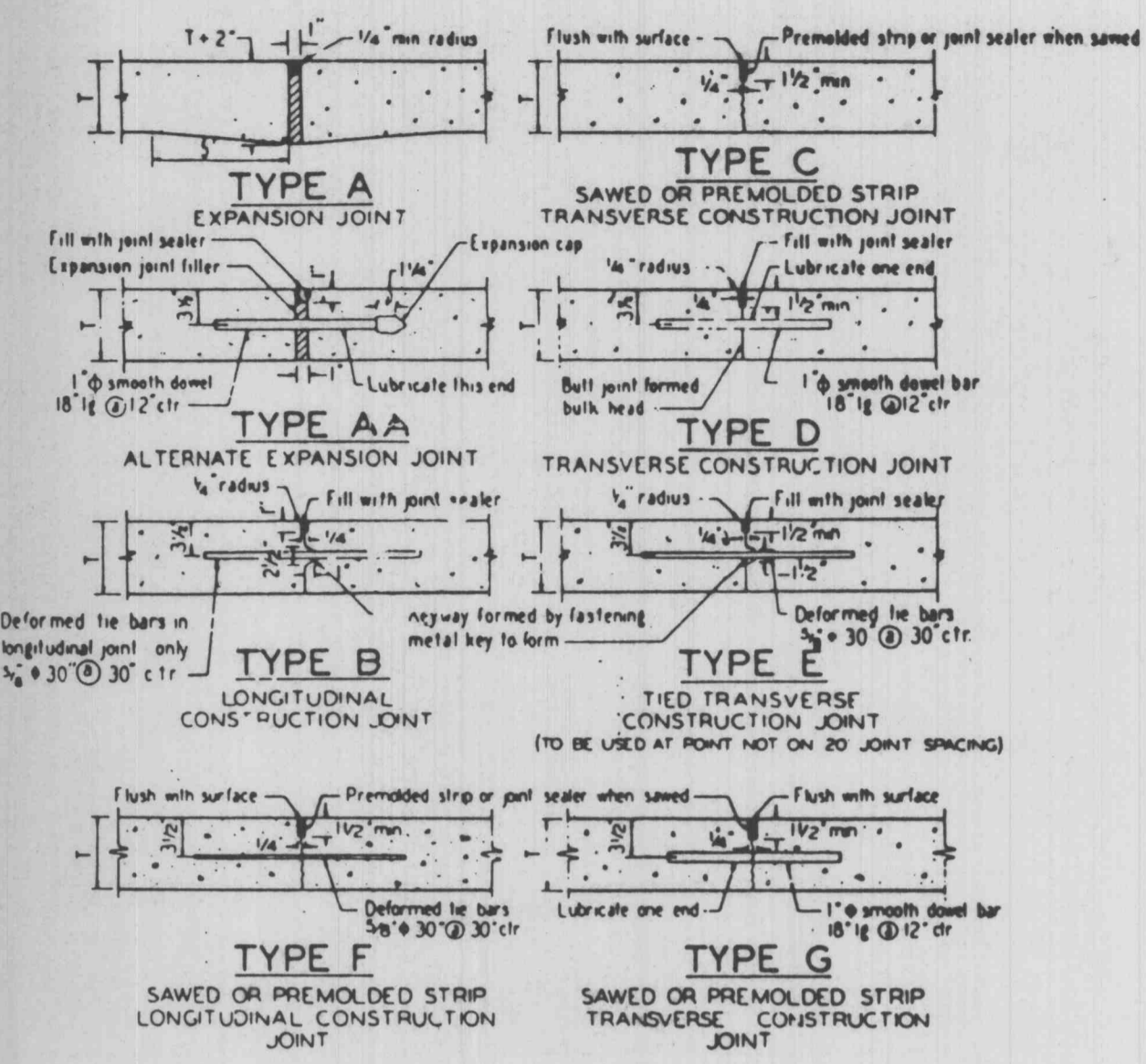
- NOTE**
- JOINT FILLER MATERIAL SHALL MEET ASTM DESIGNATIONS AS FOLLOWS:
- BITUMINOUS TYPE.....ASTM D-994
 - NON-EXTRUDING & RESILIENT BITUMINOUS TYPE (FIBER).....ASTM D-1751
 - NON-EXTRUDING & RESILIENT NON BITUMINOUS TYPE (RUBBER).....ASTM D-1752

8 PLAN OF JOINT LOCATION

WHERE VARIABLE WIDTH OF INTERSECTING PAVEMENTS ARE SHOWN JOINT SPACING MUST CONFORM TO LOCATIONS INDICATED ON STANDARD TYPICAL SECTION, INTERSECTION CHANNELIZATION DETAILS OR THE APPROVED CONSTRUCTION PLANS.



9 NOTE SEE SITE PLAN FOR DRIVEWAY WIDTHS & RADII STATE ENTRANCE DETAIL



GENERAL NOTES

- THE CONTRACTOR SHALL VISIT THE WORK SITE TO FAMILIARIZE HIMSELF AND TO ASCERTAIN BY INSPECTION PERTINENT LOCAL CONDITIONS SUCH AS LOCATION, CHARACTER AND ACCESSIBILITY OF THE SITE, AVAILABILITY OF FACILITIES, LOCATION AND CHARACTER OF EXISTING WORK WITHIN OR ADJACENT THEREON, LABOR CONDITIONS, ETC.
- ALL ELEVATIONS ARE BASED ON AN ASSUMED 100.00 DATUM. THE STORE'S FIRST FLOOR, ASSUMED AT 100.00 ELEVATION.
- PROPOSED CONTOURS SHOWN ARE FINISHED GRADES AND READ TO TOP OF PAVEMENT AND FINISHED DIRT GRADE.
- TOPOGRAPHIC SURVEY MADE BY KBR ENGINEERING, INC.
- PROPERTY INFORMATION AND DIMENSIONS SHOWN TAKEN FROM PLANS PREPARED BY GILLUM & POLK ASSOCIATES CONSULTING ENGINEERS, 144 WEST LOCKWOOD AVENUE, ST. LOUIS, MO 63119, DATED FEBRUARY 1, 1983. OUTBOUNDARY SURVEY DIMENSIONS AND BEARINGS SHOWN ON GILLUM-POLK PLAN TAKEN FROM SURVEY PREPARED BY THE STERLING ENGINEERING AND SURVEY COMPANY, 3460 HOLLEBERG DRIVE, BRIDGETON, MO 63066, DATED JANUARY 22, 1983.
- DRIVEWAY ENTRANCES:
 - OLD U.S. HIGHWAY 40 DRIVEWAY ENTRANCES SHALL MEET THE "DESIGN STANDARDS" AND "STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION" OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION AND THE CITY OF O'FALLON, MISSOURI.
- JOB CONDITIONS:
 - PROTECTION OF EXISTING IMPROVEMENTS: -PROVIDE BARRICADES, COVERINGS, OTHER TYPE PROTECTORS NECESSARY TO PREVENT DAMAGE TO EXISTING IMPROVEMENTS INDICATED ON DRAWINGS TO BE LEFT IN PLACE.
 - PROTECTION OF PEDESTRIAN AND VEHICULAR TRAFFIC WAYS: -CONTRACTOR TO PROVIDE AND INSTALL BARRICADES, SLOPE PROTECTORS, AND OTHER TYPES OF PROTECTORS AS REQUIRED TO INSURE THAT THE EARTHWORK OPERATIONS DO NOT INTERFERE WITH OR STOP PEDESTRIAN AND VEHICULAR TRAFFIC, AND SUCH THAT UNAUTHORIZED PEDESTRIANS OR VEHICLES CANNOT PHYSICALLY ENTER THE WORK AREA. ALL PROTECTIVE DEVICES TO CONFORM TO APPLICABLE CODE REQUIREMENTS. -CONTRACTOR TO INSTALL WARNING LIGHTS OR FOGS NEAR PEDESTRIAN AND VEHICULAR TRAFFIC WAYS SUCH THAT POSSIBLE HAZARDOUS AREAS WILL BE CLEARLY DEFINED DURING DARK HOURS.
 - PROTECTION OF EXISTING UTILITIES: -EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS FOR THE CONTRACTOR'S CONVENIENCE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL EXISTING UTILITY LOCATIONS EITHER SHOWN OR NOT SHOWN ON THE PLANS. CONTRACTOR IS TO IMMEDIATELY NOTIFY THE APPLICABLE UTILITY COMPANY OF ANY DAMAGES AND CAUSE REPAIRS TO THE DAMAGED UTILITY IN ACCORDANCE WITH THE REQUIREMENTS OF THE UTILITY COMPANY.
- REMOVAL OF EXISTING CONCRETE CURB, SIDEWALK AND TRUCK RAMP AND ASPHALT PAVEMENT AND CURB:
 - REMOVAL OF EXISTING PAVEMENT AND CURBING TO LIMITS SHOWN ON PLANS.
 - REMOVE FROM THE SITE ALL DEBRIS, RUBBISH, AND OTHER MATERIALS RESULTING FROM THE REMOVAL OPERATIONS. STORAGE OR SALE OR REMOVED MATERIAL WILL NOT BE PERMITTED ON THE SITE.
 - TRANSFER ALL MATERIALS REMOVED DURING PAVEMENT REMOVAL OPERATION AND DISPOSE OF OFF SITE AT AN APPROVED STATE DUMP SITE.
- COMPACTION OF SOIL:
 - ALL SOILS WHICH PROVIDE SUPPORT FOR DRIVEWAYS AND PARKING LOT AREAS SHOULD BE COMPACTED TO A MINIMUM OF 90 PERCENT OF THE MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR TEST, ASTM D-1557.
 - IF RAIN FALLS ON THE SUBGRADE PRIOR TO THE COMPLETION OF PAVING, IT MAY BE NECESSARY TO ALLOW THE SUBGRADE TO DRY BEFORE TRUCK TRAFFIC IS AGAIN ALLOWED TO TRAVEL ON THE SUBGRADE. SUBGRADE MAY BE VERY SLOW TO DRY AND PAVING OPERATIONS SIGNIFICANTLY DELAYED. THEREFORE, PROOFROLLING AND PLACEMENT OF THE SUBGRADE SHOULD BE DONE AS CLOSE TO THE PLANNED DATE OF PAVING AS IS PRACTICAL. THIS WILL HELP TO AVOID PROBLEMS RELATED TO INCLEMENT WEATHER AND HEAVY CONSTRUCTION TRAFFIC ON THE SUBGRADE, WHICH CAN CAUSE RUTTING.
- CONCRETE PAVEMENT:
 - ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH SECTION 502 OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION", DATED, 1990, AND THE CITY OF O'FALLON, MO. -PORTLAND CEMENT CONCRETE PAVEMENT: INSTALLED TO THICKNESS AS INDICATED ON THE PLANS AND IN ACCORDANCE WITH SECTION 502 OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION", DATED, 1990, AND THE CITY OF O'FALLON, MO. -CONCRETE CURING MATERIALS AND MATERIAL FOR JOINTS SHALL BE IN ACCORDANCE WITH SECTION 502 OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION", DATED, 1990, AND THE CITY OF O'FALLON, MO. USE TYPE 1, CLEAR OR TRANSPARENT WITHOUT DYE, SECTION 1055 OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION", DATED, 1990, AND THE CITY OF O'FALLON, MO. -JOINT SEALERS IN ACCORDANCE WITH SECTION 1057 OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION", DATED, 1990, AND THE CITY OF O'FALLON, MO. -PROTECTIVE COATING FOR TREATMENT OF PORTLAND CEMENT CONCRETE: BOILED LINSEED OIL MIXTURE IN ACCORDANCE WITH ASBTO DESIGNATION: M-233-86. THE COMPOUND SHALL CONSIST OF A HOMOGENEOUS MIXTURE OF 50 PERCENT BOILED LINSEED OIL AND 50 PERCENT PETROLEUM SPIRITS BY VOLUME. FOR NEW CONCRETE SURFACES THE CONCRETE SHOULD BE ALLOWED TO DRY FOR A PERIOD NOT LESS THAN 30 DAYS PRIOR TO THE APPLICATION OF THE COMPOUND.
 - THE CONTRACTOR SHALL SUBMIT A PLAN TO THE ENGINEER SHOWING DETAILS AND LOCATION FOR ALL PAVEMENT JOINTS PRIOR TO CONSTRUCTION.
- CONCRETE CURBS AND SIDEWALK:
 - CONCRETE CURBS: ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY OF O'FALLON, MO, STANDARDS AND 1990 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, SECTION 608.
 - CONCRETE SIDEWALK: ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY OF O'FALLON, MO, STANDARDS AND 1990 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, SECTION 609.
- ASPHALT PAVEMENT:
 - ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH SECTIONS 201, 301 AND 403 OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION", DATED 1990 AND THE CITY OF O'FALLON, MO. -SUBGRADE COMPACTION SHALL BE IN ACCORDANCE WITH SECTION 210. -SUBGRADE PREPARATION SHALL BE IN ACCORDANCE WITH SECTION 209. -PLANT MIX BITUMINOUS BASE COURSE IN ACCORDANCE WITH SECTION 301. -ASPHALTIC CONCRETE PAVEMENT, TYPE C IN ACCORDANCE WITH SECTION 403.
- PAINT STRIPING:
 - THIS WORK SHALL CONSIST OF PAVEMENT STRIPING, EITHER BY PAINTING OR BY LINE MARKING TAPE. ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH 1990 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, SECTION 620.
- SEEDING:
 - ALL BARE SLOPES SHALL BE HYDRAULIC SEEDING AND FERTILIZED IN ACCORDANCE WITH THE 1990 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, SECTION 605. (TYPE OF SEED AND FERTILIZER USED TO BE SUBMITTED TO OWNER AND ENGINEER BY CONTRACTOR BEFORE INSTALLING).
- QUALITY ASSURANCE:
 - CODES AND STANDARDS: -CONTRACTOR TO PERFORM WORK IN COMPLIANCE WITH APPLICABLE REQUIREMENTS OF GOVERNING AUTHORITIES.
- PERMITS:
 - CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED BY GOVERNING AUTHORITIES.
- SODDING:
 - ALL WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH SECTION 803, "SODDING", OF THE MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION, "MISSOURI STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION", DATED 1990.

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DETAIL 7 & SODDING NOTE PER CITY

DETAILS & GENERAL NOTES

PROJECT NO. 93-13

DATE 5-24-93

SHEET 4 OF 4