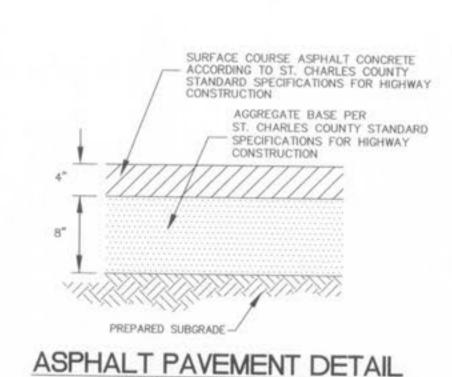
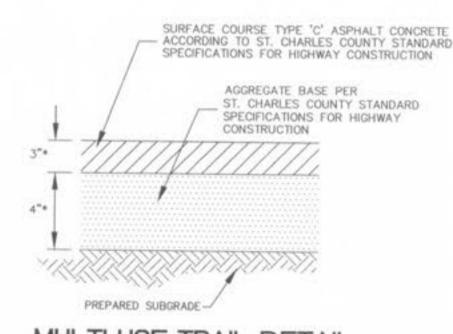


TRASH PAD



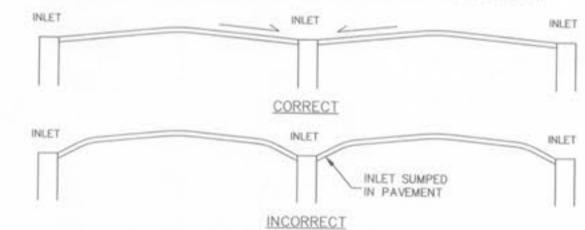


MULTI USE TRAIL DETAIL (LIGHT-DUTY) *PAVEMENT SECTION FOR ASPHALT TRAIL

PAVEMENT SECTIONS REFERENCED IN THESE PLANS HAVE NOT BEEN DESIGNED BY CEDC, INC. FOR THIS PROJECT. THEY REPRESENT PAVEMENT SECTIONS USED ON SIMILAR PROJECTS. HOWEVER, EACH PROJECT IS UNIQUE AND REQUIRES ITS OWN ANALYSIS BY A GEOTECHNICAL ENGINEER. THEREFORE, CEDC,INC. RECOMMENDS A GEOTECHNICAL ENGINEER DESIGN THE PROPOSED PAVEMENT SECTIONS FOR THIS PROJECT

PARKING AND MAIN DRIVE AREAS

PARKING / PAVEMENT AREAS SHALL BE PAVED WITH UNIFORMLY SMOOTH TRANSITIONS BETWEEN HIGH POINTS (OR DRAINAGE BREAK POINTS) IN THE PARKING LOTS TO THE LOW POINTS WHERE THE INLETS ARE LOCATED. THE CONDITION WHERE INLETS ARE SHOWN "SUMPED" IN THE PARKING LOT WITH LARGE ELEVATION DIFFERENCES IN THE PAVEMENT BEING CONSTRUCTED WITHIN THE IMMEDIATE PROXIMITY OF THE INLET SHALL BE AVOIDED.



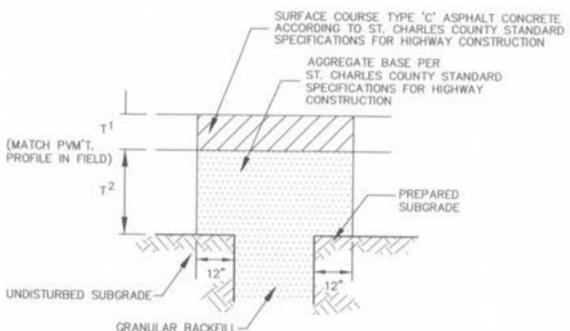
NOTE TO PAVEMENT CONTRACTOR

AGGREGATE BASE PER ST. CHARLES COUNTY STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (MATCH PVM'T. PROFILE IN FIELD) - PREPARED SUBGRADE UNDISTURBED SUBGRADE -GRANULAR BACKFILL-

> PAVEMENT SECTION TO SOUND BASE. EXCAVATE RECTANGULAR OR TRAPEZOIDAL PATCHES, EXTENDING 12 INCHES INTO ADJACENT SOUND PAVEMENT, UNLESS OTHERWISE INDICATED. CUT EXCAVATION FACES VERTICALLY, REMOVE EXCAVATED MATERIAL. RECOMPACT EXISTING

REQUIRED FOR PROPOSED UTILITIES.

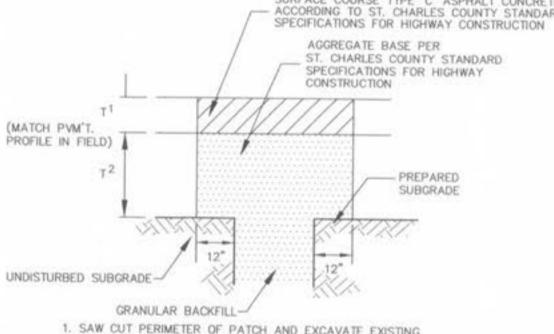
(n.t.s.)



1. SAW CUT PERIMETER OF PATCH AND EXCAVATE EXISTING UNBOUND AGGREGATE BASE COURSE TO FORM NEW SUBGRADE.

2. NEW PAVEMENT SHALL MATCH THE EXISTING PAVEMENT TYPE AND THICKNESS 3. PAVEMENT TRENCH REMOVAL IS NOT ANTICIPATED UNLESS

PATCHING ASPHALT PAVEMENT



1/2" PREMOLDED EXPANSION JOINT FILLER W/ JOINT SEALER SHADED PORTION OF CONCRETE SIDEWALK SHALL BE 7" THICK 24"x48" TRUNCATED DOME DETECTABLE WARNING SURFACE MEETING CURRENT-ADA REQUIREMENTS (ARMOR-TILE CAST IN PLACE TILE OR APPROVED EQUAL)

NOT TO SCALE

(SEE PLANS)

PROVIDE 1/2" PREMOLDED EXPANSION JOINT FILLER AT 20' CENTERS PROVIDE TOOLED CONTROL JOINTS AT 1X WIDTH.

TOOLED CONTROL JOINT

2-3/4" ANCHORING PINS 30" LONG

FRONT VIEW

PRE-CAST WHEEL STOP

6"x6"xW1.4xW1.4

WELDED WIRE FABRIC -

#4 AT 48"C VERT .-

1-#4 TOP AND BOTTOM-(STOP REINF, AT JOINTS)

(n.t.s.)

SURFACE COURSE ASPHALT CONCRETE ACCORDING TO ST. CHARLES COUNTY STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION AGGREGATE BASE PER ST. CHARLES COUNTY STANDARD CONCRETE TO BE 3,500 p.s.i. IF CAST IN PLACE, 4000 PSI-PREPARED SUBGRADE -

SPECIFICATIONS FOR HIGHWAY CONSTRUCTION 1.) REFER TO SITE PLAN FOR AREAS OF PAVEMENT TYPE AND THICKNESS. 2.) EXPANSION MATERIAL SHALL BE INSTALLED EVERY 20' IN CURB. EXPANSION MATERIAL SHALL BE OF A TYPE EQUAL TO THE ASPHALT IMPREGNATED FIBERBOARD CONFORMING TO ASTM C 175 FOR EXTERIOR WORK, EXPANSION MATERIAL SHALL EXTEND FULL DEPTH OF CURB AND GUTTER, LESS

CURBS TO BE PROVIDED WITH SAW CUT OR TOOLED JOINT AT 10' INTERVALS.

JOINTS AND SAW CUTS TO BE SEALED WITH LIMESTONE COLORED SEALANT.

1/4" AT TOP OF CONCRETE.

CONCRETE VERTICAL CURB AND PAVEMENT SECTION NOT TO SCALE

2" MIN. TYPE 1 AGGREGATE-

-FINISH WITH 3/4"

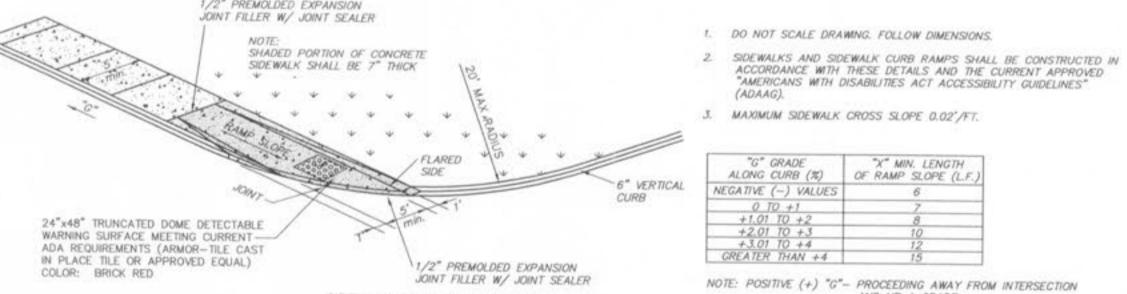
PREPARED EARTH

SUBGRADE

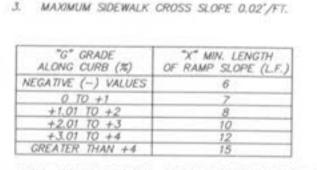
RADIUS TOOL

CONTRACTION JOINTS EQUAL TO WIDTH 5" CONCRETE SIDEWALK 1/2" EXPANSION JOINT LENGTH OF SIDEWALK WHERE IT ABUTS EXISTING STRUCTURES, PROVIDE -1/2" EXPANSION JOINT -PREMOLDED FILLER WITH BACKER ROD AND SEALANT. PLAN 1 1/2"-1-1/4" 1/2"-1-1/2" CONTRACTION JOINT -PREMOLDED 4" TYPE 1 AGGREGATE ON JOINT FILLER PREPARED SUBGRADE (TYP) 6"x6"xW1.4xW1.4-WELDED WIRE FABRIC SEALANT ON TOP OF THE PREMOLDED

SECTION A-A JOINT FILLER DETAIL OF CONCRETE SIDEWALK



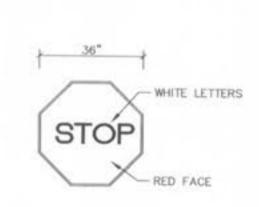
MONOLITHIC CONCRETE CURB & WALK



NOTE: POSITIVE (+) "G"- PROCEEDING AWAY FROM INTERSECTION AND UP A GRADE. SIDEWALK AT BACK OF CURB NEGATIVE (-) "G"- PROCEEDING AWAY FROM INTERSECTION STRAIGHT CURB RAMP - 6" VERTICAL CURB AND DOWN A GRADE.

TYPICAL PARKING STALLS





STOP SIGN

PAINT GRAPHIC ON PAVING

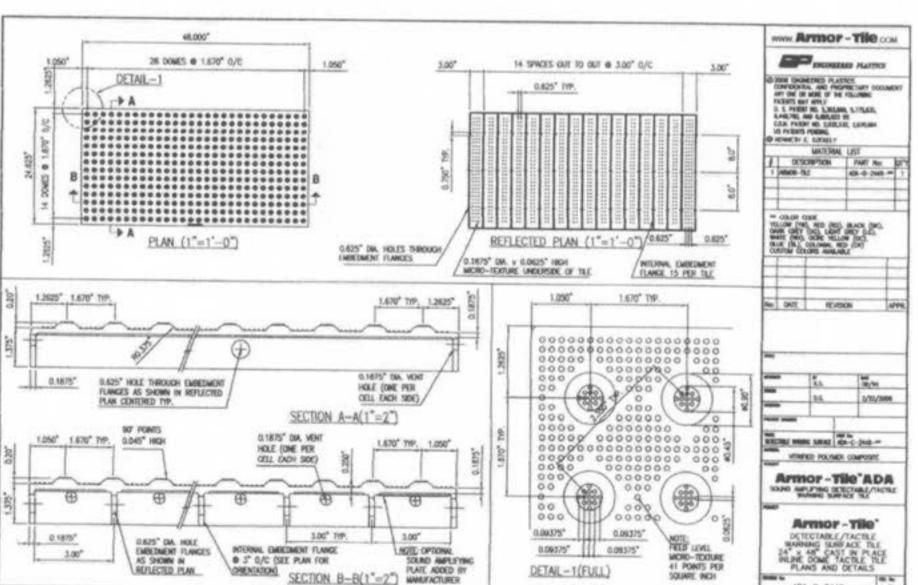
HANDICAP SIGN

(n.t.s.)

@ H.D.C.P. STALLS

PAINTED BLUE

ALL SIGN POST, BACKS AND BRACKET ARMS SHALL BE PAINTED BLACK USING CARBOLINE RUSTBOND PENETRATING SEALER SG AND CARBOLINE 133 HB PAINT (OR EQUIVALENT AS APPROVED BY THE CITY).



DETECTABLE WARNING SURFACE

DO NOT SCALE DRAWING, FOLLOW DIMENSIONS.

SIDEWALKS AND SIDEWALK CURB RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THESE DETAILS AND THE CURRENT APPROVED "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES"

 PROVIDE A LANDING AT THE TOP OF EACH STRAIGHT RAMP WHEN THE GRADE ALONG CURB ("G") IS GREATER THAN +2% AND LESS THAN +7%. FOR OTHER VALUES OF "G", INCLUDING ALL NEGATIVE (-) VALUES, NO LANDING IS REQUIRED.

MINIMUM SIDEWALK WIDTH ALONG 6" VERTICAL CURB SHALL BE 5 FEET. MINIMUM SIDEWALK WIDTH ALONG 3" ROLLED CURB SHALL BE 4 FEET.

5. MAXIMUM SIDEWALK CROSS SLOPE 0.02'/FT.

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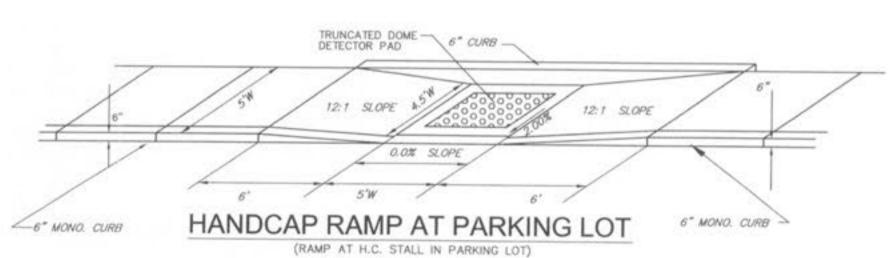
6. ALL SIDEWALK SECTIONS SHALL BE 4" THICK, EXCEPT WHERE INDICATED OTHERWISE BY SHADED PORTIONS SHOWN ON DETAILS.

WHERE CURB RAMP MEETS PAVEMENT, BULLNOSE WILL NOT BE

8. CONSTRUCT A DIAGONAL RAMP WHEN THE MAXIMUM CORNER RADIUS ALLOWED FOR A STRAIGHT RAMP IS EXCEEDED.

IF INTEGRAL CONCRETE CURB IS CONSTRUCTED, STRIKE A DUMMY JOINT ACROSS BOTTOM OF RAMP AT CURB LINE. IF CONCRETE CURB IS DOWELED-ON, BLOCK OUT PAVEMENT TO PROVIDE FULL DEPTH CURB ACROSS RAMP FROM OUTER POINT OF CURB TAPER TO OUTER POINT OF

10. FOR PAVEMENT LONGITUDINAL AND TRANSVERSE JOINTS AND DOWEL AND TIE BAR REQUIREMENTS AND DIMENSIONS, REFER TO THE PAVEMENT CONSTRUCTION DETAILS FOR "JOINTS AND CURBS", ST. LOUIS COUNTY STANDARD DRAWING C502.03.



Proj. # 0708 No. Description Date City Submittal 10/08/07 per City 11/14/07 Permit Set 01/23/08

BRANDON ASHER HARF

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Plans

lmpr

BRANDON A. HARP, P.E.

Site & Pavement Details

Planning and Development No. 2507 Planning and Zoning Commission Approval - October 4, 2007