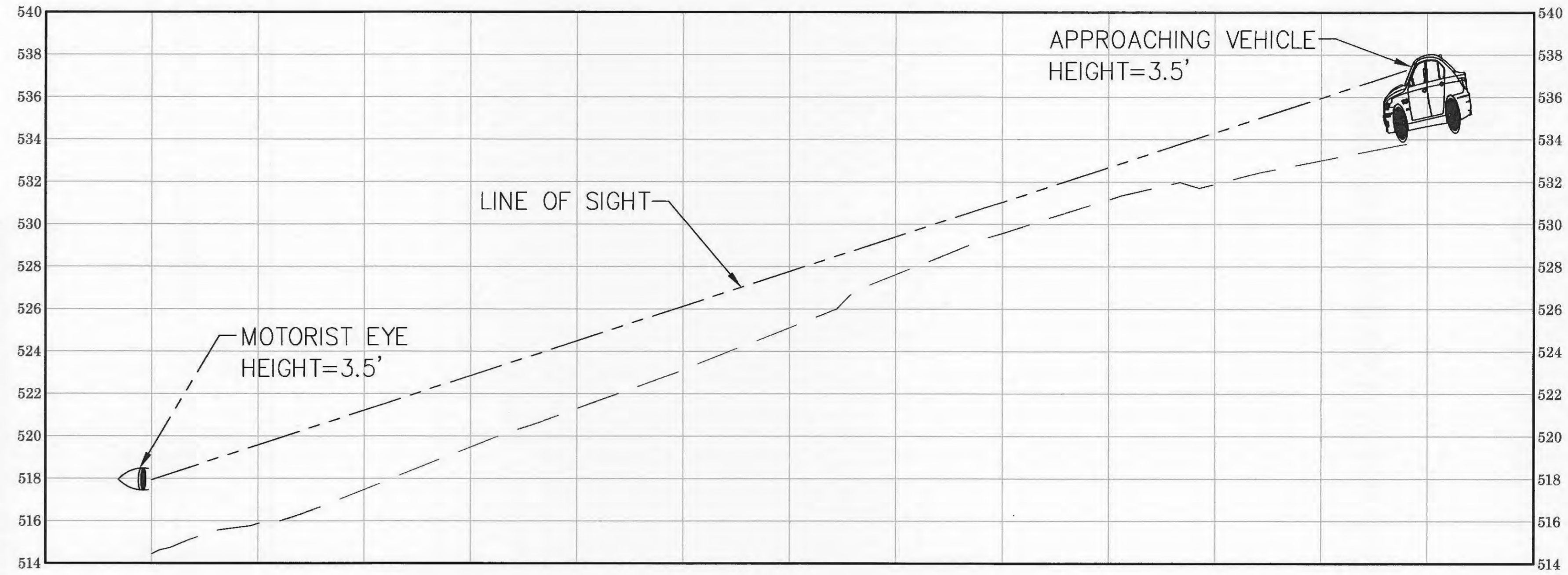
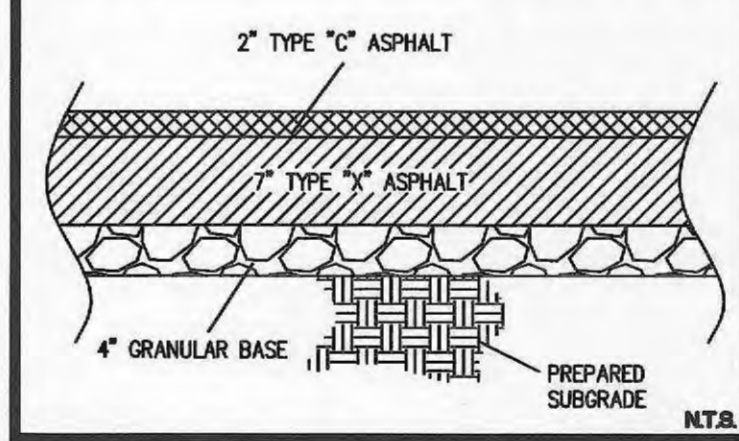


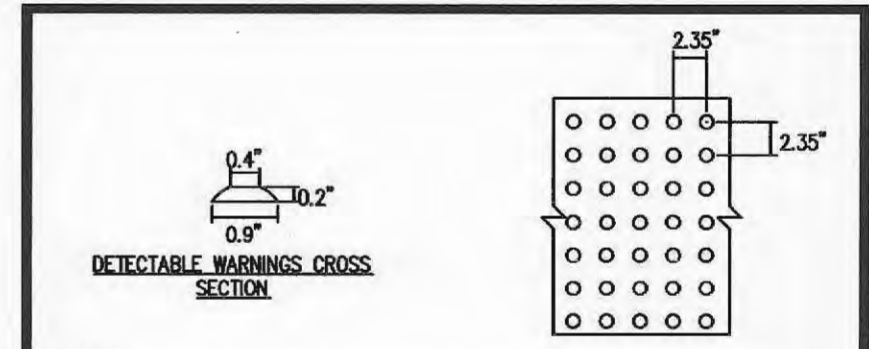
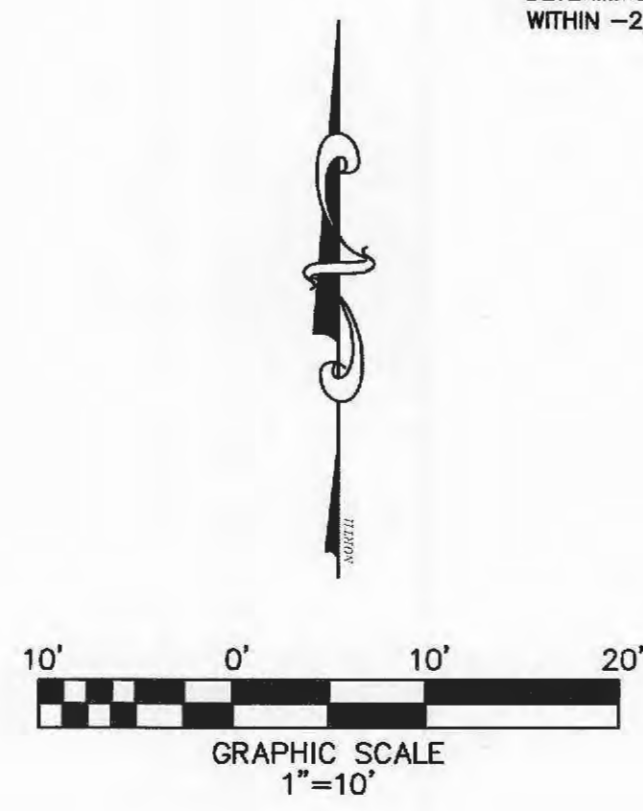
BRAMBLETT ROAD & BRAMBLETT CROSSING



ASPHALT PAVEMENT (W/IN RIGHT-OF-WAY)



1. ASPHALT PAVEMENT SHALL BE COMPACTED TO 98% MAXIMUM DENSITY.
2. SUB GRADE AND BASE BENEATH PAVEMENTS SHALL BE COMPACTED TO ST. CHARLES COUNTY HIGHWAY DEPARTMENT SPECIFICATIONS. THE MOISTURE RANGE SHALL BE DETERMINED BY THE STANDARD OR MODIFIED PROCTOR DENSITY METHOD AASHTO T-99 AND WITHIN -2/+4 PERCENTAGE POINTS OF THE OPTIMUM MOISTURE CONTENT.



- NOTES:
1. DETECTABLE WARNINGS SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE.
 2. THE DETECTABLE WARNINGS SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE OR OTHER POTENTIAL HAZARD IS 6 TO 8 INCHES FROM THE CURB LINE OR OTHER POTENTIAL HAZARD, SUCH AS A REFLECTING POOL EDGE OR THE DYNAMIC ENVELOPE OF RAIL OPERATIONS.
 3. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
 4. THERE SHALL BE A MINIMUM OF 70 PERCENT CONTRAST IN LIGHT REFLECTANCE BETWEEN THE DETECTABLE WARNING AND AN ADJOINING SURFACE, OR THE DETECTABLE WARNING SHALL BE "SAFETY YELLOW". THE MATERIAL USED TO PROVIDE VISUAL CONTRAST SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING SURFACE.

TRUNCATED DOMES

ALL CURB RAMPS SHALL HAVE TRUNCATED DOMES IN ACCORDANCE WITH ADAAG.

