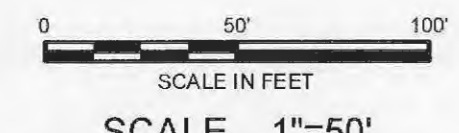


The signed and sealed original of this drawing is on file at the offices of The Clayton Engineering Company, Inc. The signed and sealed original is the official document and shall take precedence over any digital version.

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Underground facilities, structures & utilities have been plotted from available surveys, records & information, and therefore, do not necessarily reflect the actual existence, nonexistence, size, type, number of, or location of these facilities, structures, & utilities.

The Contractor shall be responsible for verifying the actual location of all underground facilities, structures, & utilities, either shown or not shown on these plans. The underground facilities, structures, & utilities shall be located in the field prior to any grading, excavation or construction of improvements. These provisions shall in no way absolve any party from complying with the Underground Facility Safety and Damage Prevention Act, Chapter 319, RSMo.



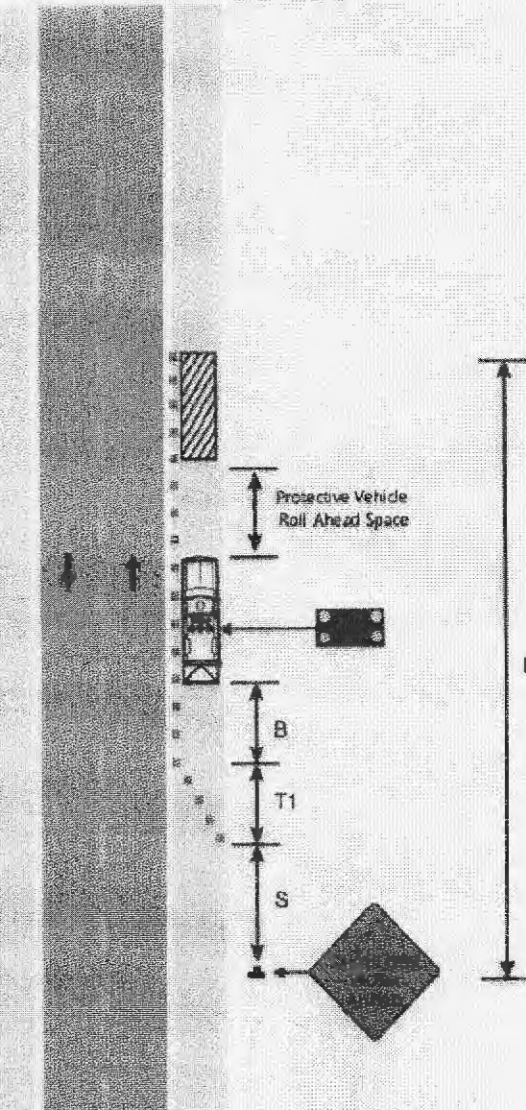
SCALE 1"=50'  
 Note: Caution should be taken in comparing the written scale to the graphic scale to ascertain if this plan has been enlarged or reduced.

**616.8-3 (TA-3) Shoulder Work on Undivided Highways - MT**

SPEED	SIGN SPACING (ft.)		TAPER LENGTH (ft.)		OPTIONAL BUFFER LENGTH (ft.) (B)	CHANNELIZER SPACING (ft.)	
	Undivided (U)	Divided (D)	Shoulder (TS)	Lane (TL)		Tapers	Buffer/Work Areas
0-35	200	-	70	-	250	35	50
40-45	350	-	150	-	300	40	100
50-55	500	-	185	-	495	50	100
60-70	1000	-	235	-	730	80	100

1 Shoulder taper length based on 10 ft. (standard shoulder width) offset. 2 Lane taper length based on 12 ft. (standard lane width) of set.

ROADWAY TYPE	SIGN HEIGHT	MAXIMUM WORK ZONE LENGTH (L)
URBAN	7' Post 5' Post	1 M.
RURAL UNDIVIDED	7' Post 5' Post	3 M.



In addition to shoulder work, this typical application is applicable to work beyond shoulder where vehicles and equipment are parked on the shoulder.

A protective vehicle shall be used while work is in progress. The protective vehicle should be equipped with a TMA and positioned at least 150 ft. in advance of the work space.

If encroachment onto driving surface occurs and there is not 10 ft. of driving surface available for the lane of traffic, that traffic lane shall be closed. Refer to appropriate lane closure typical applications.

If an arrow panel is used for an operation on the shoulder, the caution mode shall be displayed.

For short duration or mobile operations, signs, channelization devices, and protective vehicles may be reduced or eliminated if a work vehicle with activated rotating lights or strobe lights is used. However, if limited sight distances exist, a protective vehicle should be used. This protective vehicle should be equipped with a TMA and truck mounted flashing arrow panel and positioned at least 150 ft. in advance of the work space or work vehicle, as applicable. If a protective vehicle is used, a vehicle mounted sign shall be mounted at a recommended height of 48 in. above the road surface.

For work beyond shoulder, where vehicles and equipment are parked on the shoulder, the protective vehicle may be eliminated if a work vehicle with activated rotating lights or strobe lights is used.

Vehicle hazard warning signals shall not be used instead of the vehicle's rotating lights or strobe lights.

Additional warning signs shall be erected at each intersection with another state highway within the work zone. Upon the discretion of the supervisor, additional warning signs may be erected at other intersections within the work zone.

Other appropriate signs may be used in lieu of SHOULDER WORK AHEAD sign.

**MODOT Traffic Control Notes**

- All lane closures within MODOT's R/W must be approved by MODOT's work zone coordinator.
- All public roads/streets must be kept clear of mud & debris at all times. Failure to do so will be cause for the City of O'Fallon and/or MODOT to suspend work.
- Protective vehicle should be used when lane closures are necessary.
- All intersecting driveways or roadways are to remain clear of channelizer cones or drums.
- All warning signs shall be placed as not to obstruct any intersecting drives or roadways.
- The Contractor is to supply all channelizers and construction signage.

**PROJECT TITLE:**  
 SITE CONSTRUCTION PLANS  
 DIRT CHEAP of O'FALLON  
 #708 MO. STATE HIGHWAY K  
 O'FALLON, MISSOURI 63366

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MO. Cert. of Authority - Professional Engineering #000007  
 & Professional Surveying #000014

Erik J. Staley - Professional Engineer  
 (PE-2006000132)

Plan Date:	November 8, 2013
Revision Date:	April 28, 2014
Revision Date:	May 22, 2014
Revision Date:	August 1, 2014
Revision Date:	Sept. 4, 2014

**Developer / Owner Information:**  
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**TRAFFIC CONTROL PLAN**

P+Z No.: (23-13.02)  
 Approval Date: 10/03/13  
 City No.

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