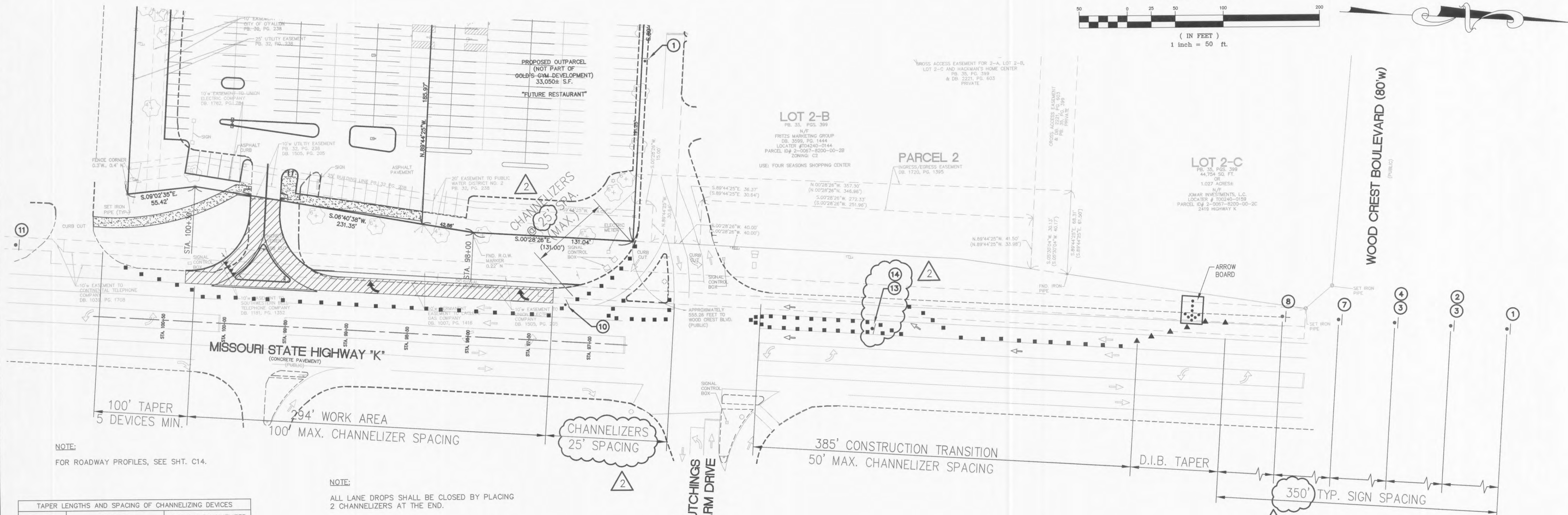


GRAPHIC SCALE



(IN FEET)
1 inch = 50 ft.



NOTE:
FOR ROADWAY PROFILES, SEE SHT. C14.

NOTE:
ALL LANE DROPS SHALL BE CLOSED BY PLACING 2 CHANNELIZERS AT THE END.

SPEED (1) MPH (S)	MINIMUM TAPER LENGTHS (L) FOR LATERAL SHIFTS (W)			MAXIMUM CHANNELIZER SPACING	
	10 FT	11 FT	12 FT	THROUGH TAPER	THROUGH WORK AREA
0-35	205 FT	225 FT	245 FT	35 FT	50 FT
40-45	450 FT	495 FT	540 FT	40 FT	100 FT
50-55	550 FT	605 FT	660 FT	50 FT	100 FT
60-70	700 FT	770 FT	840 FT	60 FT	100 FT

SPEED (1) MPH (S)	SIGN SPACING	
	NON-DIVIDED HIGHWAYS	DIVIDED HIGHWAYS
0-35	200 FT	200 FT
40-45	350 FT	500 FT
50-55	500 FT	1000 FT
60-70	1000 FT	1000 FT

TAPER LENGTH (L)
 L = W x S FOR 45 MPH OR MORE
 L = 100 FOR 40 MPH OR LESS
 L = TAPER LENGTH IN FEET
 W = LATERAL SHIFT IN FEET
 S = POSTED SPEED IN MPH

DEGREE OF CURVE	RADIUS	SPACING ON CURVE	SPACING IN ADVANCE OF AND BEYOND CURVE		
			1ST	2ND	3RD
TANGENT TO 1/2"	11,500	100	100	100	100
1/2" TO 2"	11,500 TO 2,880	75	85	100	100
2" TO 4"	2,880 TO 1,430	60	70	80	100
4" TO 8"	1,430 TO 715	50	60	70	80
8" TO 16"	715 TO 360	40	50	60	70
16" TO 40"	360 TO 140	20	30	50	70

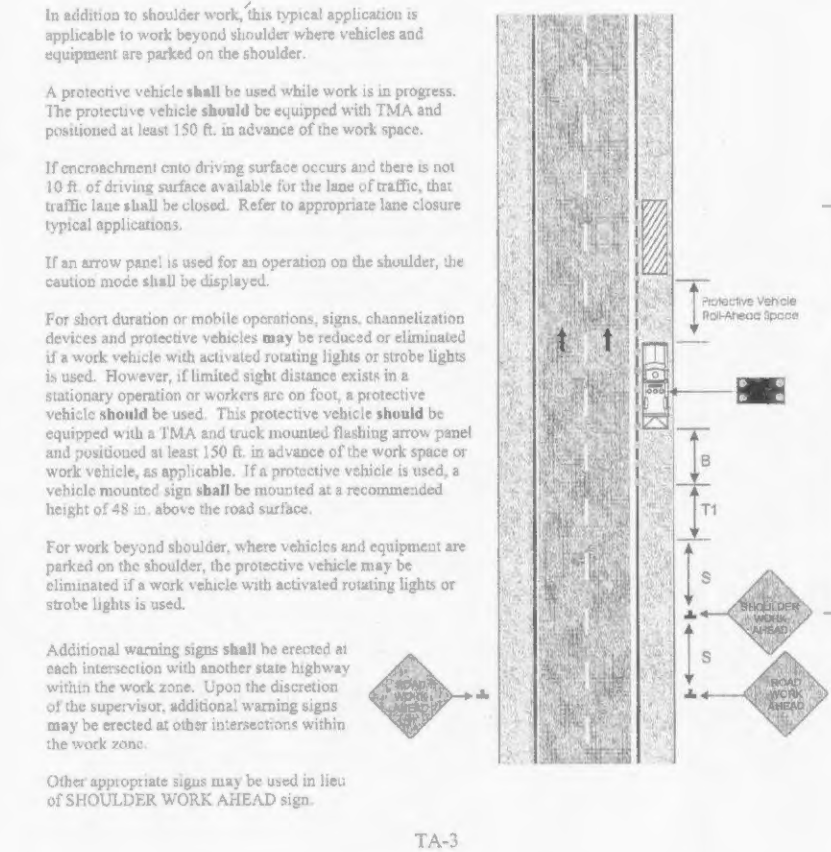
SPEED (1) MPH (S)	MINIMUM TAPER LENGTHS FOR LANE WIDTHS (4)			END TREATMENT (5)
	10 FT	11 FT	12 FT	
> 40	160 FT	168 FT	176 FT	BARRIER HEIGHT TRANSITION
45 <	160 FT	168 FT	176 FT	APPROVED CRASH CUSHION

- NOTES:
DIMENSIONS IN FEET UNLESS OTHERWISE NOTED.
- SPACING BETWEEN SIGNS AND SPACING BETWEEN LAST SIGN AND FLAGGER, SPACING OF TAPER, OR SIGNED CONDITION.
 - SPACING MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS.
 - TAPER LENGTHS SHOWN INCLUDE LENGTH REQUIRED FOR LANE AND 10 FT SHOULDER.
 - CONCRETE BARRIER MAY BE INSTALLED AT AN 8:1 FLARE RATE FROM THE SHOULDER POINT TO THE LIMITS OF THE CLEAR ZONE WHERE THE SIDE SLOPE IS 6:1 OR FLATTER.

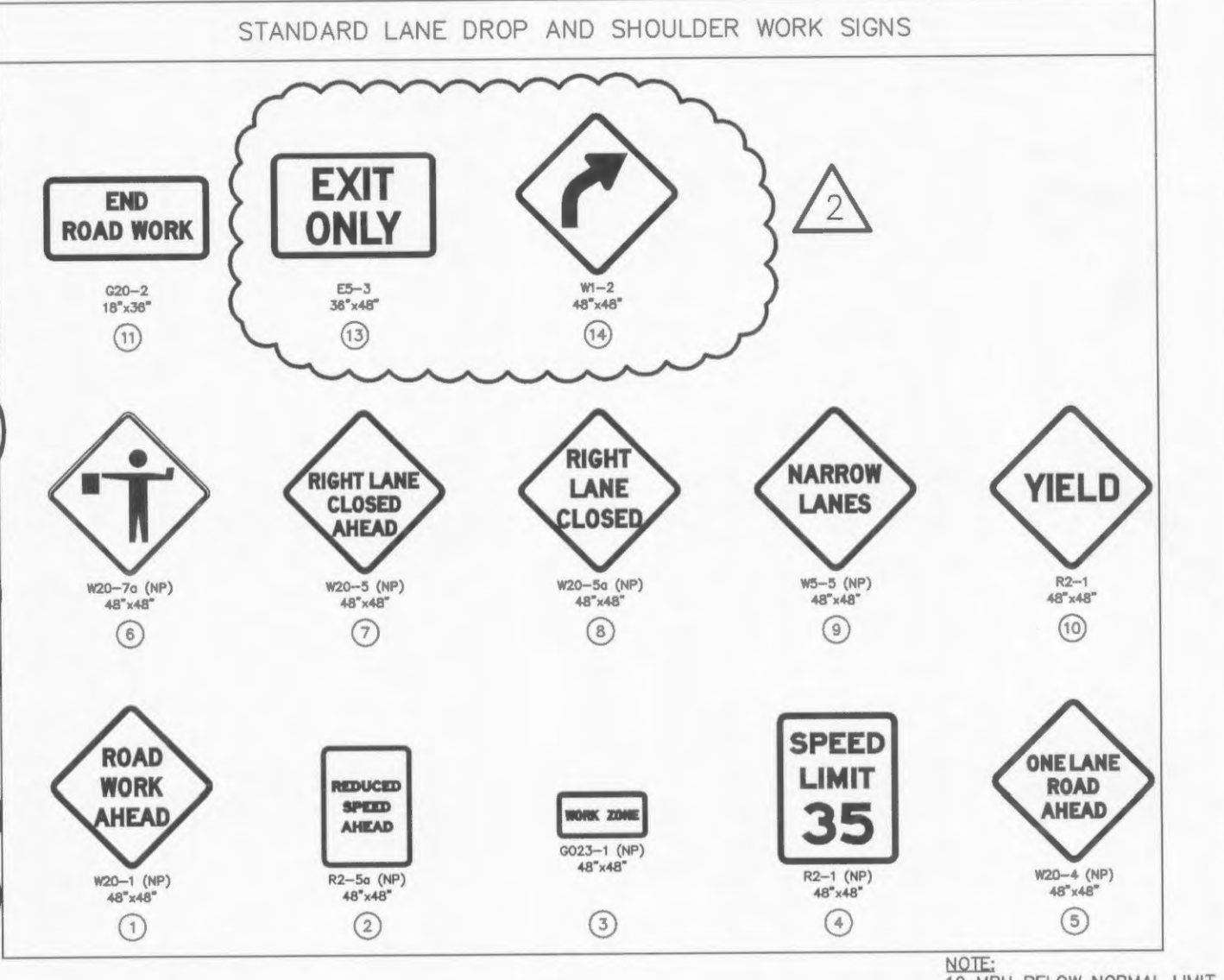
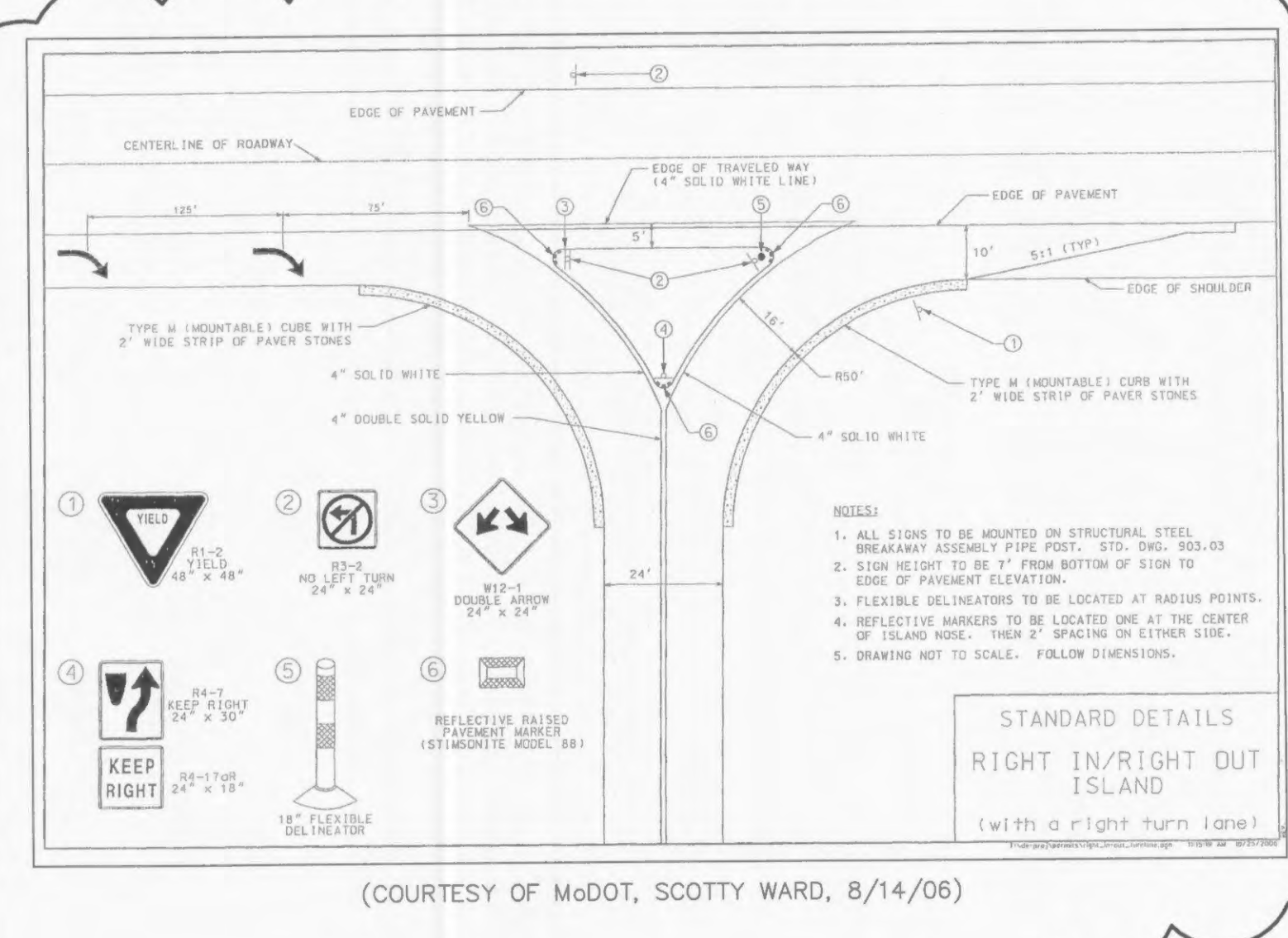
- GENERAL TRAFFIC CONTROL PLAN (TCP) NOTES:**
- TRAFFIC CONTROL SHALL BE MAINTAINED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE MISSOURI DEPARTMENT OF TRANSPORTATION STANDARD PLAN 616.10 FOR TRAFFIC CONTROL DEVICES. CONTRACTOR SHALL VERIFY TRAFFIC CONTROL WITH MDT INSPECTOR REGARDLESS OF WHAT IS SHOWN ON THIS PLAN.
 - PAVEMENT MARKINGS SHALL BE MAINTAINED IN ACCORDANCE WITH PART VI OF THE MUTCD.
 - ANY EXISTING SIGNS WHICH CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED.
 - NO DIRECT PAYMENT SHALL BE MADE FOR RELOCATION OF SIGNS. ANY MODIFICATION TO THIS TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.
 - SPACING MAY BE ADJUSTED AS NECESSARY TO MEET FIELD CONDITIONS, A MINIMUM OF 200' MAY BE USED WHERE CONDITIONS DICTATE.
 - ALL EXISTING SIGNS AND MARKERS WITHIN THE LIMITS OF OPERATIONS SHALL BE REMOVED OR RELOCATED IN ACCORDANCE WITH SECTION 104.10.2 OF THE 2004 MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
 - ALL SIDE ROADS AND ENTRANCES ARE TO BE CONSTRUCTED SO THAT ACCESS IS MAINTAINED AT ALL TIMES, UNLESS OTHERWISE NOTED IN THIS TCP.
 - LANE CLOSURES AND REDUCED SPEED CONTROL SIGNS ARE REMOVED WHEN WORK IS INACTIVE. MOVE CHANNELIZERS TO SHOULDER AND RESTORE ALL LANES OF TRAFFIC WHEN WORK IS INACTIVE.

SPEED (1)	MINIMUM SPACING (2)	TAPER LENGTH (3)	OPTIONAL BUFFER LENGTH (4)	CHANNELIZER SPACING (5)	WORK AREA (6)
0-35	200	75	50	30	40
40-45	350	100	75	40	50
50-55	500	150	100	50	75
60-70	1000	225	150	60	100

ROADWAY	SIGN HEIGHT	MAXIMUM WORK ZONE LENGTH (L)
Urban	7' Post	1.0M
RURAL DIVIDED	7' Post	2.0M

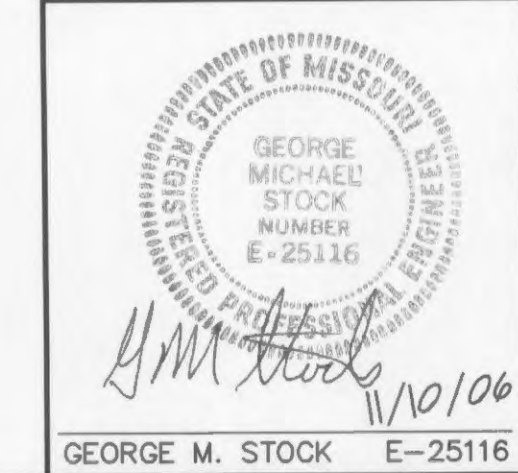


- TRAFFIC CONTROL LEGEND**
- SIGN (SINGLE SIDED)
 - SIGN (DOUBLE SIDED)
 - FLAGGER
 - DIRECTIONAL INDICATOR BARRICADE (D.I.B.)
 - CHANNELIZER
 - TEMPORARY CONCRETE TRAFFIC BARRIER (C.T.B.)
 - MOVEABLE BARRICADE
 - SAND FILLED IMPACT ATTENUATOR
 - WORK AREA
 - TRAFFIC ARROW TO BE MARKED ON PAVEMENT
 - LANE USAGE
 - SWT 4" SOLID WHITE TAPE
 - INT 4" INTERMITTENT WHITE TAPE
 - DSY 4" DOUBLE SOLID YELLOW TAPE
 - SMP 4" SOLID WHITE PAINT
 - IMP 4" INTERMITTENT WHITE PAINT
 - DSYP 4" DOUBLE SOLID YELLOW PAINT



MoDOT, WATER COMMENTS 11/10/06
 CITY, MoDOT, FIRE COMMENTS 11/7/06

GOLD'S GYM - O'FALLON, MO
TRAFFIC CONTROL PLAN



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DRAWN BY: H.L.T. DATE: 8/14/06
 CHECKED BY: C.A.M. DATE: 8/14/06
 JOB NUMBER: 206-3850
 SHEET: C18