

SUPERELEVATION DATA
(ELEVATION ARE TO TOP OF CURB)

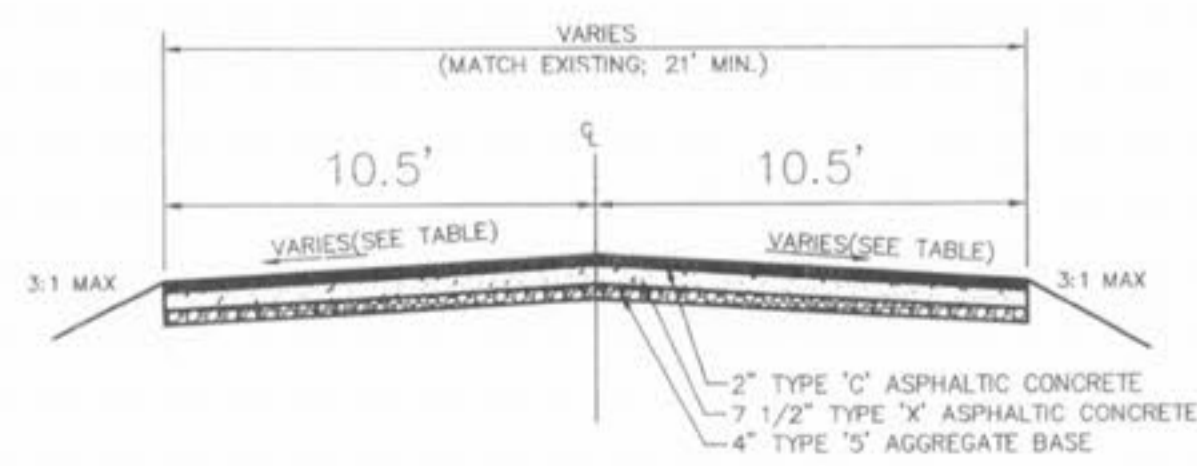
STATION	DESCRIPTION	CENTERLINE ELEVATION	LEFT SIDE GRADE	LEFT SIDE ELEV.	RIGHT SIDE GRADE	RIGHT SIDE ELEV.
STA. 0+50.00	TRANSITION	469.14		SEE WARPING DETAIL		
STA. 0+75.00	TRANSITION	469.84		SEE WARPING DETAIL		
STA. 0+88.58	TRANSITION (P.C.)	470.23		SEE WARPING DETAIL		
STA. 1+00.00	TRANSITION	470.55	+1.46%	471.48	-2.00%	470.58
STA. 1+25.00	TRANSITION	471.24	+2.46%	472.46	-2.46%	471.18
STA. 1+50.00	TRANSITION	471.83	+3.46%	473.32	-3.46%	471.55
STA. 1+63.58	BEGIN FULL 4% SUPER	472.10	+4.00%	473.73	-4.00%	471.71
STA. 1+75.00	FULL 4% SUPER	472.30	+4.00%	473.91	-4.00%	471.92
STA. 2+00.00	FULL 4% SUPER	472.66	+4.00%	474.22	-4.00%	472.30
STA. 2+25.00	FULL 4% SUPER	472.91	+4.00%	474.40	-4.00%	472.57
STA. 2+26.33	END FULL 4% SUPER	472.92	+4.00%	474.41	-4.00%	472.58
STA. 2+50.00	TRANSITION	473.04	+3.05%	474.23	-3.05%	472.91
STA. 2+75.00	TRANSITION	473.07	+2.05%	473.99	-2.05%	473.16
STA. 3+00.00	TRANSITION	472.98	+1.05%	473.68	-2.00%	473.09
STA. 3+01.33	TRANSITION (P.T.)	472.97	+1.00%	473.67	-2.00%	473.08
STA. 3+25.00	TRANSITION	472.77	+0.05%	*473.37	-2.00%	472.88
STA. 3+50.00	TRANSITION	472.46	-0.95%	*472.77	-2.00%	472.57
STA. 3+75.00	TRANSITION	472.09	-1.95%	*472.21	-2.00%	472.20
STA. 3+76.33	END TRANSITION	472.07	-2.00%	*472.18	-2.00%	472.18

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SUPERELEVATION DATA
(ELEVATION ARE TO TOP OF CURB)

STATION	DESCRIPTION	CENTERLINE ELEVATION	LEFT SIDE GRADE	LEFT SIDE ELEV.	RIGHT SIDE GRADE	RIGHT SIDE ELEV.
STA. 10+79.58	TRANSITION	492.09	-2.00%	492.20	-2.00%	492.20
STA. 11+00.00	TRANSITION	492.50	-1.18%	492.77	-2.00%	492.61
STA. 11+25.00	TRANSITION	493.00	-0.18%	493.46	-2.00%	493.11
STA. 11+50.00	TRANSITION	493.50	+0.82%	494.16	-2.00%	493.61
STA. 11+54.58	TRANSITION (P.C.)	493.59	+1.00%	494.29	-2.00%	493.70
STA. 11+75.00	TRANSITION	494.00	+1.82%	494.85	-2.00%	494.11
STA. 12+00.00	TRANSITION	494.50	+2.82%	495.55	-2.82%	494.45
STA. 12+25.00	TRANSITION	495.00	+3.82%	496.24	-3.82%	494.76
STA. 12+29.58	BEGIN FULL 4% SUPER	495.09	+4.00%	496.37	-4.00%	494.81
STA. 12+50.00	FULL 4% SUPER	495.50	+4.00%	496.78	-4.00%	495.22
STA. 12+75.00	FULL 4% SUPER	496.01	+4.00%	497.29	-4.00%	495.73
STA. 13+00.00	FULL 4% SUPER	496.54	+4.00%	497.82	-4.00%	496.26
STA. 13+25.00	FULL 4% SUPER	497.09	+4.00%	498.37	-4.00%	496.81
STA. 13+50.00	FULL 4% SUPER	497.65	+4.00%	498.93	-4.00%	497.37
STA. 13+75.00	FULL 4% SUPER	498.24	+4.00%	499.52	-4.00%	497.96
STA. 14+00.00	FULL 4% SUPER	498.84	+4.00%	500.12	-4.00%	498.56
STA. 14+25.00	FULL 4% SUPER	499.44	+4.00%	500.72	-4.00%	499.16
STA. 14+29.93	END FULL 4% SUPER	499.56	+4.00%	500.84	-4.00%	499.28
STA. 14+50.00	TRANSITION	500.05	+3.20%	501.17	-3.20%	499.93
STA. 14+75.00	TRANSITION	500.65	+2.20%	501.58	-2.20%	500.72
STA. 15+00.00	TRANSITION	501.26	+1.20%	501.99	-2.00%	501.37
STA. 15+04.93	TRANSITION (P.T.)	501.38	+1.00%	502.08	-2.00%	501.49
STA. 15+25.00	TRANSITION	501.86	+0.20%	502.40	-2.00%	501.97
STA. 15+50.00	TRANSITION	502.47	-0.80%	502.81	-2.00%	502.58
STA. 15+75.00	TRANSITION	503.07	-1.80%	503.22	-2.00%	503.18
STA. 15+79.93	END TRANSITION	503.19	-2.00%	503.30	-2.00%	503.30

*TOP OF CURB ELEVATIONS PROVIDED HOWEVER CURB IS NOT PROPOSED IN THIS AREA DUE TO INTERSECTION.



CONNECTION TO EXISTING KOCH ROAD ASPHALT PAVEMENT DETAIL
NO SCALE

SUPERELEVATION DATA
(ELEVATION ARE TO TOP OF CURB)

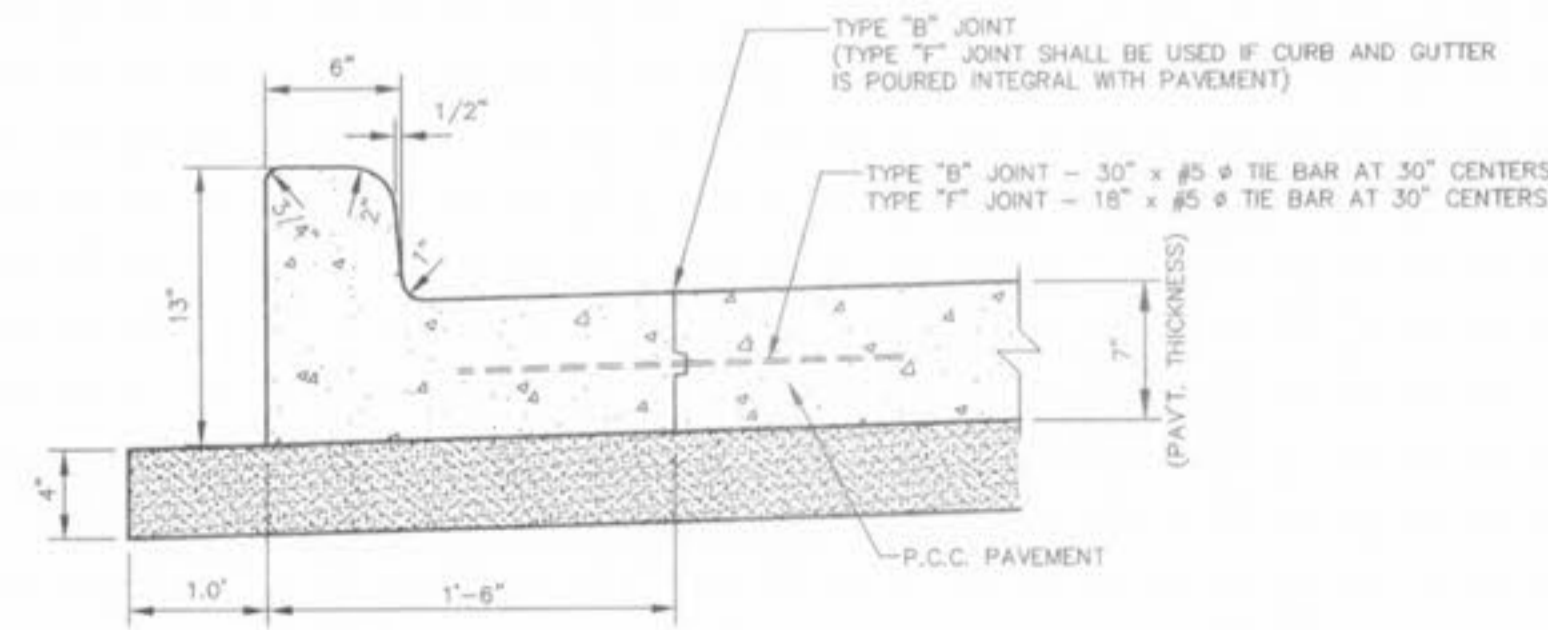
STATION	DESCRIPTION	CENTERLINE ELEVATION	LEFT SIDE GRADE	LEFT SIDE ELEV.	RIGHT SIDE GRADE	RIGHT SIDE ELEV.
STA. 3+76.33	BEGIN TRANSITION	472.07	-2.00%	*472.18	-2.00%	472.18
STA. 4+00.00	TRANSITION	471.84	-2.00%	471.95	-1.05%	472.14
STA. 4+25.00	TRANSITION	471.71	-2.00%	471.82	-0.05%	472.11
STA. 4+50.00	TRANSITION	471.70	-2.00%	471.81	+0.95%	472.39
STA. 4+51.33	TRANSITION (P.C.)	471.70	-2.00%	471.81	+1.00%	472.40
STA. 4+75.00	TRANSITION	471.82	-2.00%	471.93	+1.95%	472.70
STA. 5+00.00	TRANSITION	472.06	-2.95%	471.98	+2.95%	473.14
STA. 5+25.00	TRANSITION	472.42	-3.95%	472.15	+3.95%	473.69
STA. 5+26.33	BEGIN FULL 4% SUPER	472.44	-4.00%	472.16	+4.00%	473.72
STA. 5+50.00	FULL 4% SUPER	472.91	-4.00%	472.63	+4.00%	474.19
STA. 5+73.10	END FULL 4% SUPER	473.47	-4.00%	473.19	+4.00%	474.75
STA. 5+75.00	TRANSITION	473.52	-3.92%	473.26	+3.92%	474.78
STA. 6+00.00	TRANSITION	474.25	-2.92%	474.18	+2.92%	475.52
STA. 6+25.00	TRANSITION	475.10	-2.00%	475.21	+1.92%	475.97
STA. 6+48.10	TRANSITION (P.T.)	476.00	-2.00%	476.11	+1.00%	476.70
STA. 6+50.00	TRANSITION	476.08	-2.00%	476.19	+0.92%	476.76
STA. 6+75.00	TRANSITION	477.18	-2.00%	477.29	-0.08%	477.52
STA. 7+00.00	TRANSITION	478.34	-2.00%	478.45	-1.08%	478.63
STA. 7+23.10	END TRANSITION	479.41	-2.00%	*479.52	-2.00%	*479.52

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SUPERELEVATION DATA
(ELEVATION ARE TO TOP OF CURB UNLESS NOTED OTHERWISE)

STATION	DESCRIPTION	CENTERLINE ELEVATION	LEFT SIDE GRADE	LEFT SIDE ELEV.	RIGHT SIDE GRADE	RIGHT SIDE ELEV.
STA. 17+59.91	BEGIN TRANSITION	507.55	-2.00%	*507.66	-2.00%	*507.66
STA. 17+75.00	TRANSITION	507.91	-2.00%	508.02	-1.25%	508.17
STA. 18+00.00	TRANSITION	508.52	-2.00%	508.63	0.0%	509.02
STA. 18+19.91	TRANSITION (P.C.)	509.00	-2.00%	509.11	+1.00%	509.70
STA. 18+25.00	TRANSITION	509.12	-2.00%	509.23	+1.25%	509.86
STA. 18+50.00	TRANSITION	509.63	-2.50%	509.64	+2.50%	510.62
STA. 18+75.00	TRANSITION	510.03	-3.75%	509.80	+3.75%	511.26
STA. 18+79.91	BEGIN FULL 4% SUPER	510.09	-4.00%	509.81	+4.00%	511.37
STA. 19+00.00	FULL 4% SUPER	510.30	-4.00%	510.02	+4.00%	511.58
STA. 19+25.00	FULL 4% SUPER	510.45	-4.00%	510.17	+4.00%	511.73
STA. 19+50.00	FULL 4% SUPER	510.48	-4.00%	510.20	+4.00%	511.76
STA. 19+55.48	END FULL 4% SUPER	510.47	-4.00%	509.72 (PAV'T.)	+4.00%	511.20 (PAV'T.)
STA. 19+75.00	TRANSITION	510.40	-3.13%	509.86 (PAV'T.)	+3.13%	510.90 (PAV'T.)
STA. 20+00.00	TRANSITION	510.19	-2.00%	509.85 (PAV'T.)	+1.88%	510.43 (PAV'T.)
STA. 20+15.48	TRANSITION (P.T.)	509.97	-2.00%	509.70 (PAV'T.)	+1.00%	510.11 (PAV'T.)
STA. 20+25.00	TRANSITION	509.86	-2.00%	509.61 (PAV'T.)	+0.83%	509.93 (PAV'T.)
STA. 20+50.00	TRANSITION	509.41	-2.00%	509.20 (PAV'T.)	-0.62%	509.31 (PAV'T.)
STA. 20+75.00	TRANSITION	509.03	-2.00%	508.83 (PAV'T.)	-1.87%	508.60 (PAV'T.)
STA. 20+75.48	END TRANSITION	509.03	-2.00%	508.82 (PAV'T.)	-2.00%	508.79 (PAV'T.)

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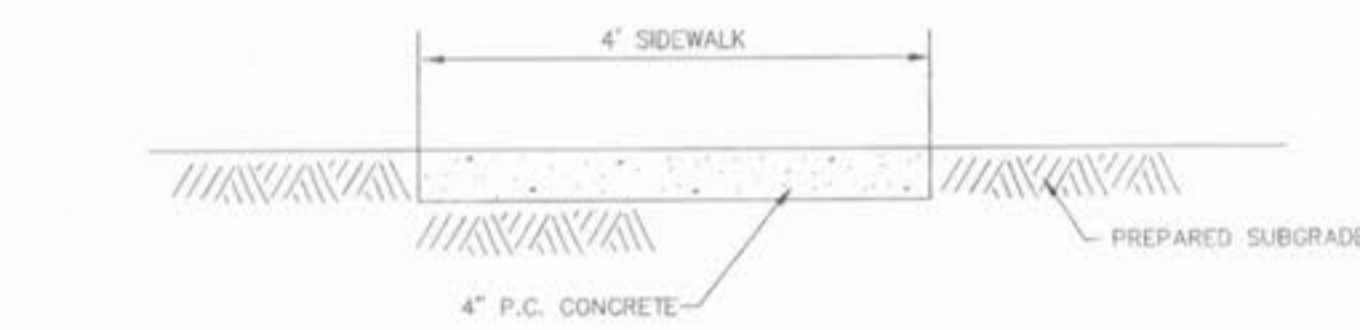
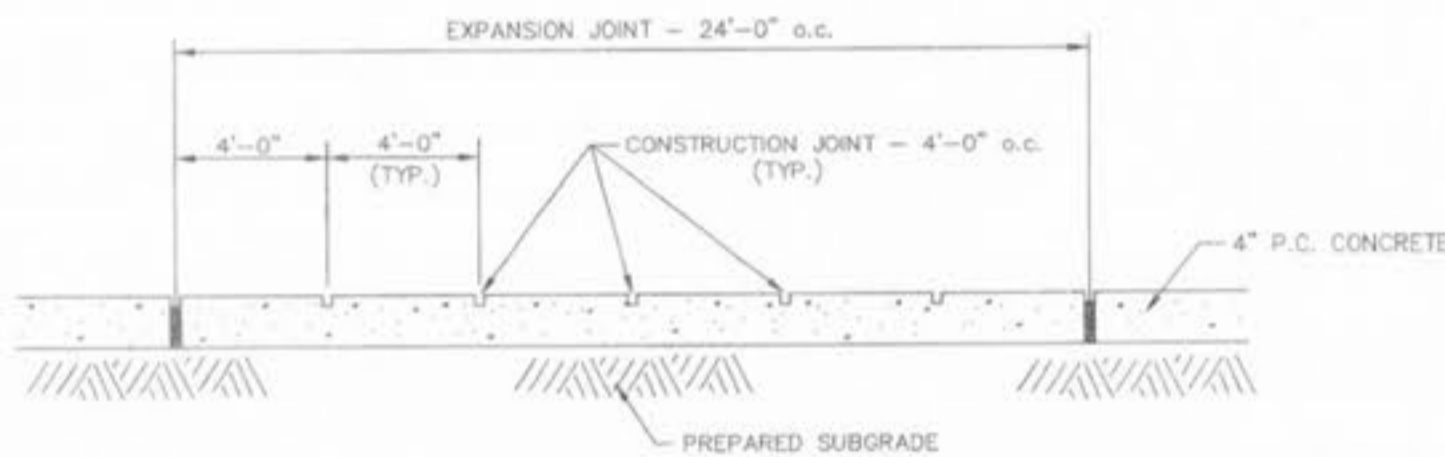
INTEGRAL VERTICAL CURB
NOT TO SCALE

NOTE:
1. ALL DEFORMED BARS FOR JOINTS AND CURBS SHALL BE BILLET STEEL BARS CONFORMING TO A.S.T.M. 615-75, GRADE 40.

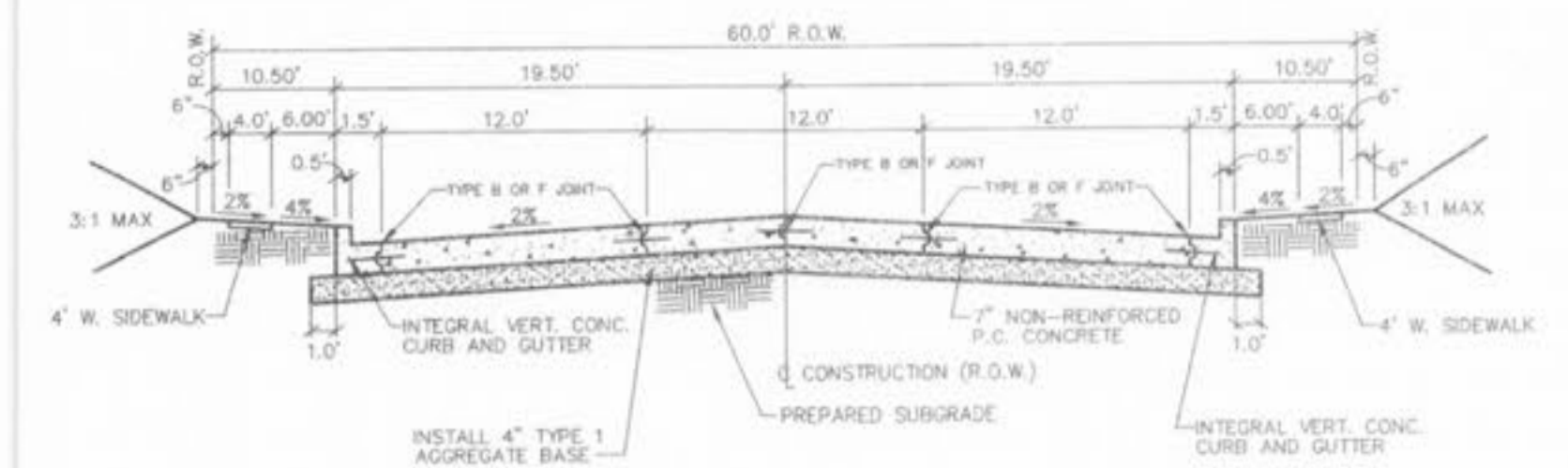
SUPERELEVATION DATA
(ELEVATION ARE TO TOP OF CURB)

STATION	DESCRIPTION	CENTERLINE ELEVATION	LEFT SIDE GRADE	LEFT SIDE ELEV.	RIGHT SIDE GRADE	RIGHT SIDE ELEV.
STA. 8+29.22	BEGIN TRANSITION	484.32	-2.00%	484.43	-2.00%	484.43
STA. 8+50.00	TRANSITION	485.28	-2.00%	485.39	-1.17%	485.55
STA. 8+75.00	TRANSITION	486.40	-2.00%	486.51	-0.17%	486.87
STA. 9+00.00	TRANSITION	487.42	-2.00%	487.53	+0.83%	488.08
STA. 9+04.22	TRANSITION (P.C.)	487.58	-2.00%	487.69	+1.00%	488.28
STA. 9+25.00	TRANSITION	488.34	-2.00%	488.45	+1.83%	489.20
STA. 9+50.00	TRANSITION	489.16	-2.83%	489.11	+2.83%	490.21
STA. 9+51.40	TRANSITION (mid-point)	489.20	-2.87%	489.14	+2.87%	490.26
STA. 9+75.00	TRANSITION	489.87	-2.00%	489.98	+1.95%	490.75
STA. 9+98.58	TRANSITION (P.T.)	490.45	-2.00%	490.56	+1.00%	*491.15
STA. 10+00.00	TRANSITION	490.48	-2.00%	490.59	+0.94%	*491.16
STA. 10+25.00	TRANSITION	491.00	-2.00%	491.11	-0.06%	*491.38
STA. 10+50.00	TRANSITION	491.50	-2.00%	491.61	-1.06%	*491.79
STA. 10+73.58	END TRANSITION	491.97	-2.00%	492.08	-2.00%	*492.08

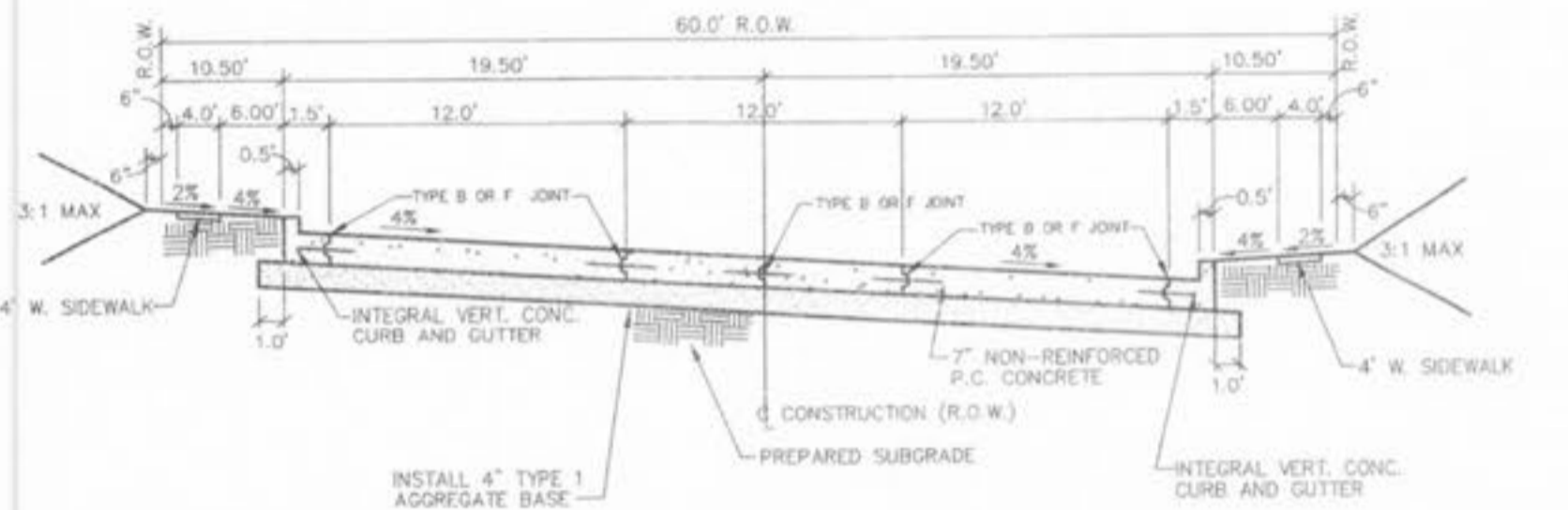
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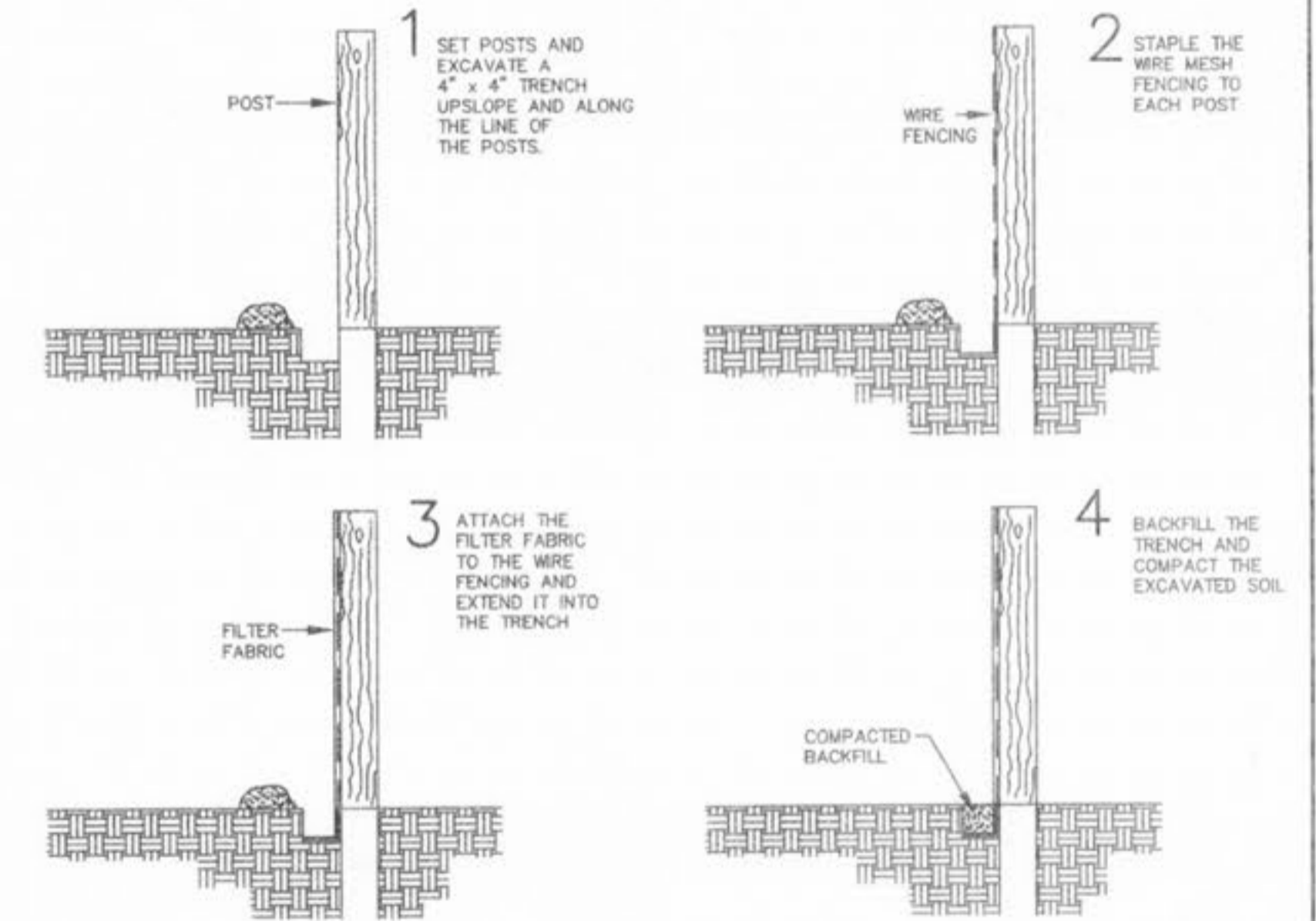
CONCRETE SIDEWALK DETAIL
NOT TO SCALE



RELOCATED KOCH ROAD COLLECTOR ROAD CONCRETE PAVEMENT DETAIL
(3% W. CONC. PAV'T.)
NO SCALE
ALL JOINTS TO BE SAWS PER CITY OF O'FALLON.



RELOCATED KOCH ROAD COLLECTOR ROAD SUPER ELEVATED CONCRETE PAVEMENT DETAIL
(3% W. CONC. PAV'T.)
NO SCALE
ALL JOINTS TO BE SAWS PER CITY OF O'FALLON.



1. FILTER BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
2. SHOULD THE FABRIC DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL BE NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
3. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN DEPOSITS REACH APPROXIMATELY HALF THE HEIGHT OF THE BARRIER.
4. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE OR FILTER BARRIER IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEED.



SILTATION FENCE DETAIL



DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
7-08-04	EB	REVISED PER CITY COMMENTS			
7-30-04	EB	REVISED PER CITY COMMENTS			
9-9-04	EP	REVISED PER CITY COMMENTS			
9-16-04	EP	REVISED PER CITY COMMENTS			

BAX ENGINEERING CO., INC.
1052 South Cloverleaf Drive
St. Peters, MO 63376-6445
Phone: 636-928-5552
Fax: 636-928-1718

PREPARED FOR:
HYLAND GREEN LLC

PROJECT NAME:
KOCH ROAD RELOCATION

PROJECT NO.: 98-102930 DATE: 05/20/2004

TYPICAL SECTIONS & DETAILS

DECLARATION OF RESPONSIBILITY:
I hereby certify that the documents intended to be authorized by my seal are limited to this sheet, and I hereby disclaim any responsibility for all other drawings, specifications, estimates, reports or other documents or instruments relating to or intended to be used for any part or parts of the architectural or engineering project or work.

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