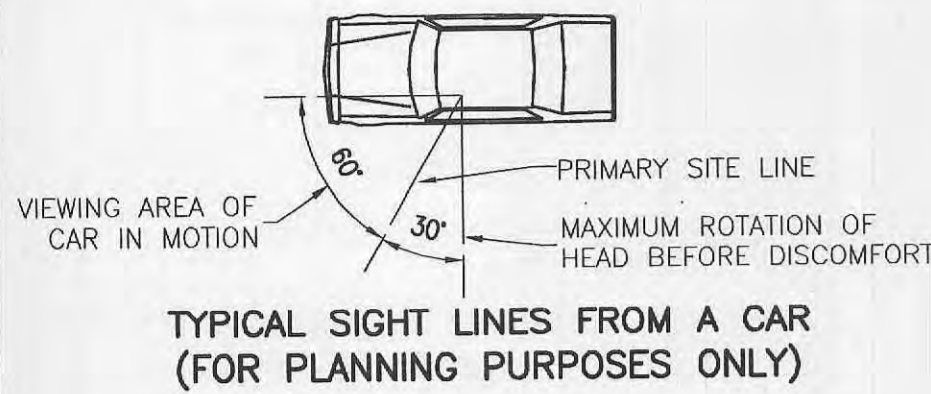


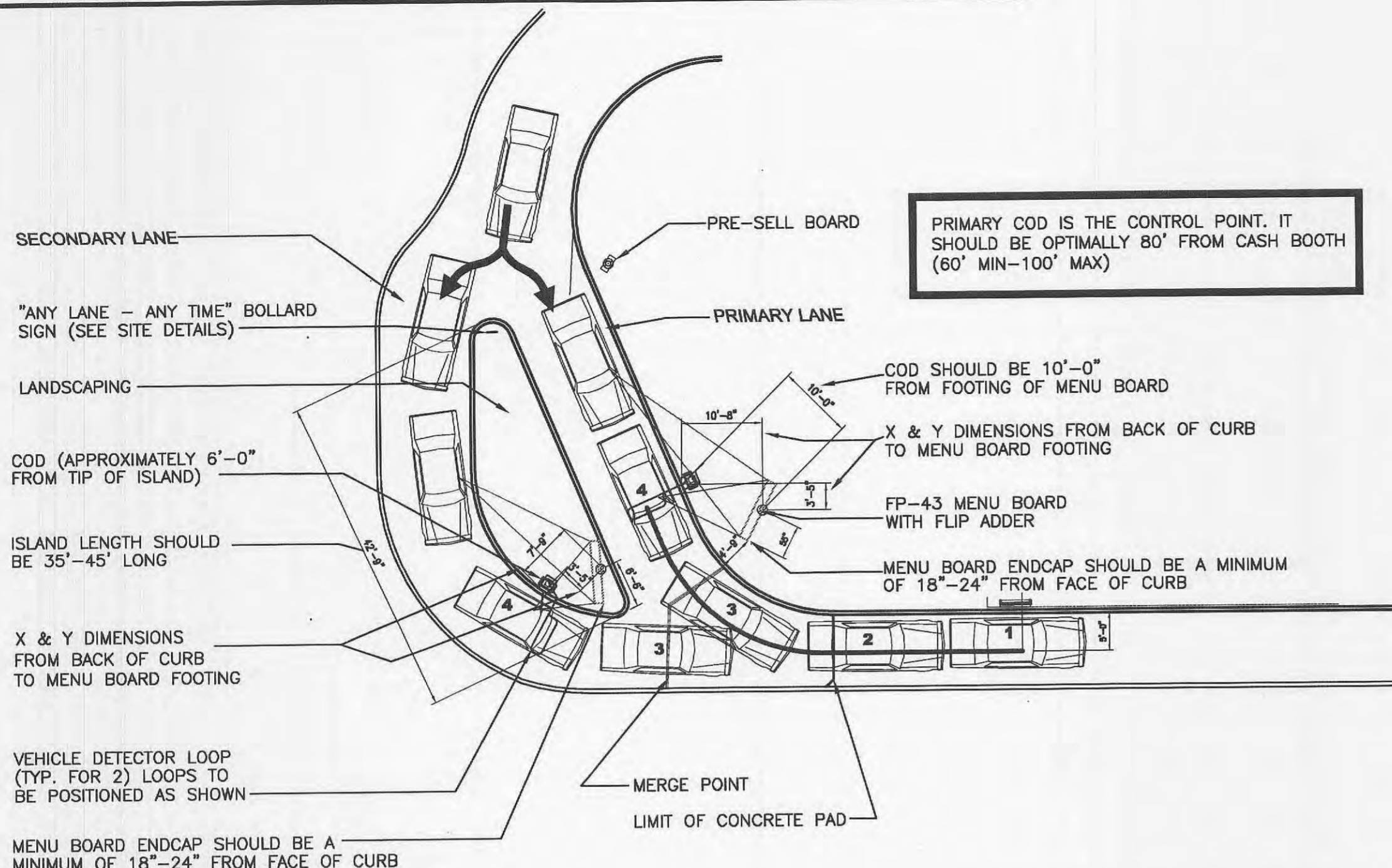
SIDE BY SIDE DRIVE THRU LAYOUT



TYPICAL SIGHT LINES FROM A CAR (FOR PLANNING PURPOSES ONLY)

GENERAL NOTES

1. THE LOCATION AND ORIENTATION OF MENU BOARD AND PRE-SELL BOARDS WERE DETERMINED BY THE SIGHT LINES OF THE CARS SHOWN. THE CARS WERE POSITIONED ACCORDING TO THE MAXIMUM AMOUNT OF CARS DURING A PEAK PERIOD.
2. THE MENU BOARD SHOULD BE ORIENTED FOR VIEWING BY CAR #4 (PRIMARY USER) AND BY CAR #5 (SECONDARY USER).
3. PRE-SELL BOARD "A" AND "B" SHOULD BE ORIENTED FOR VIEWING BY CAR #6.
4. ONE (1) DETECTOR LOOP SHOULD BE LOCATED AT THE C.O.D. TERMINAL.
5. CONDUIT WIRING MUST BE LOCATED WITHIN 2'-0" OF BASE.
6. COIL 3'-0" OF WIRE AT THE BASE OF EACH FOUNDATION.
7. THE ANGLE AND LOCATION OF THE BOARDS SHOULD BE VERIFIED WITH THE PROJECT MANAGER PRIOR TO INSTALLATION.



PRIMARY COD IS THE CONTROL POINT. IT SHOULD BE OPTIMALLY 80' FROM CASH BOOTH (60' MIN-100' MAX)

1. THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE PRIMARY COD MUST BE A MIN OF 10'-0" AND A MAX. OF 15'-0". THIS MEASUREMENT IS TAKEN PARALLEL TO THE INSIDE CURB FACE OF THE PRIMARY LANE.
2. THE CENTER OF THE SECONDARY MENU BOARD FOOTING SHOULD BE LOCATED 22" FROM FACE OF CURB AND APPROXIMATELY 6'-0" FROM THE TIP OF THE CURBED ISLAND.
3. THE MENU BOARD SHOULD BE AT AN ANGLE BETWEEN 40° AND 50° FROM A CAR POSITIONED AT THE COD.
4. THE END CAP OF THE SECONDARY LANE MENU BOARD SHOULD BE 18" FROM FACE OF CURB.
5. THE OVERALL LENGTH OF THE CURBED ISLAND SHOULD BE 35'-45". THIS ALLOWS FOR THREE CARS IN EACH LANE, AS COUNTED BACK FROM THE CAR AT THE COD. A THIRD CAR CAN BE POSITIONED AT THE COMMITMENT POINT, WHICH IS THE LAST POINT AT WHICH A CUSTOMER CAN COMMIT TO A LANE.
6. THE DISTANCE BETWEEN THE TIP OF THE CURBED ISLAND AND THE CENTER LINE OF THE SECONDARY COD MUST BE A MIN. OF 10'-0" AND A MAX. OF 13'-0". THIS MEASUREMENT IS TAKEN ALONG THE CENTERLINE OF THE SECONDARY LANE WHILE FOLLOWING THE INSIDE CURB FACE. WHEN THE COD IS LOCATED IN THE 11'-13' RANGE, THE LOOP DETECTOR IS TO BE 2' FORWARD FROM THE CENTER OF THE COD.
7. DRIVE-THRU LANES BOUND BY CURB ON BOTH SIDES ARE TO BE 12'-0" WIDE AS MEASURED FROM CURB FACES. DRIVE-THRU LANES BOUND BY CURB ON ONE SIDE AND PAINTED STRIPING ON THE OTHER MUST BE A MIN. OF 10'-0" WIDE AS MEASURED FROM CURB FACE TO PAINT.
8. ALL PRE-SELL BOARDS AND BOLLARD SIGNS MUST BE 18"-24" FROM FACE OF CURB. THIS IS MEASURE FROM THE CLOSEST POINT ON THE BOARD OR SIGN. THE DISTANCE BETWEEN THE PRIMARY COD AND PRE-SELL BOARD IS TO BE 30' (± 3').
9. THE MIN. RADIUS FOR ALL INSIDE/DRIVER'S SIDE DRIVE-THRU CURBING IS 20'-0".
10. PRE-SELL BOARDS ARE TO BE ORIENTED AT AN ANGLE OF 30° FROM THE CURB. IF LOCATED ON A TURN, THEN THE BOARD SHOULD BE 30° FROM A CAR POSITIONED AT THAT BOARD.
11. AUGER COD FOOTINGS TO BE TIGHT AGAINST BACK OF CURB. FACE OF COD IS TO BE 6" FROM BACK OF CURB.
12. THE CENTER OF THE PRIMARY LANE MENU BOARD'S FOOTING IS TO BE 10' FROM BACK OF CURB. THIS WILL LEAVE THE FACE OF THE MENU BOARD 9' FROM BACK OF CURB.

THIS DETAIL IS FOR REFERENCE ONLY! REFER TO SHEET C1.2 FOR DRIVE THRU LAYOUT DIMENSIONS AND LAYOUT. EXACT LOCATIONS SHOULD BE VERIFIED WITH PROJECT MANAGER.

NOTES:

1. GENERAL CONTRACTOR TO COORDINATE THE RESPONSIBILITIES OF THE ELECTRIC CONTRACTOR, SIGN SUPPLIER, AND AUDIO SUPPLIER.
2. GENERAL CONTRACTOR TO INSTALL PRE-FORMED, PRE-WIRED VEHICLE DETECTOR LOOP.
3. #12 GREEN GROUNDING CONDUCTOR MUST BE RUN WITH CIRCUIT CONDUCTORS FROM GROUND BUS IN REFERENCE BOARDS.
4. ALL UNDERGROUND CONDUIT FOR AUDIO TO BE P.V.C., NO METAL ALLOWED.

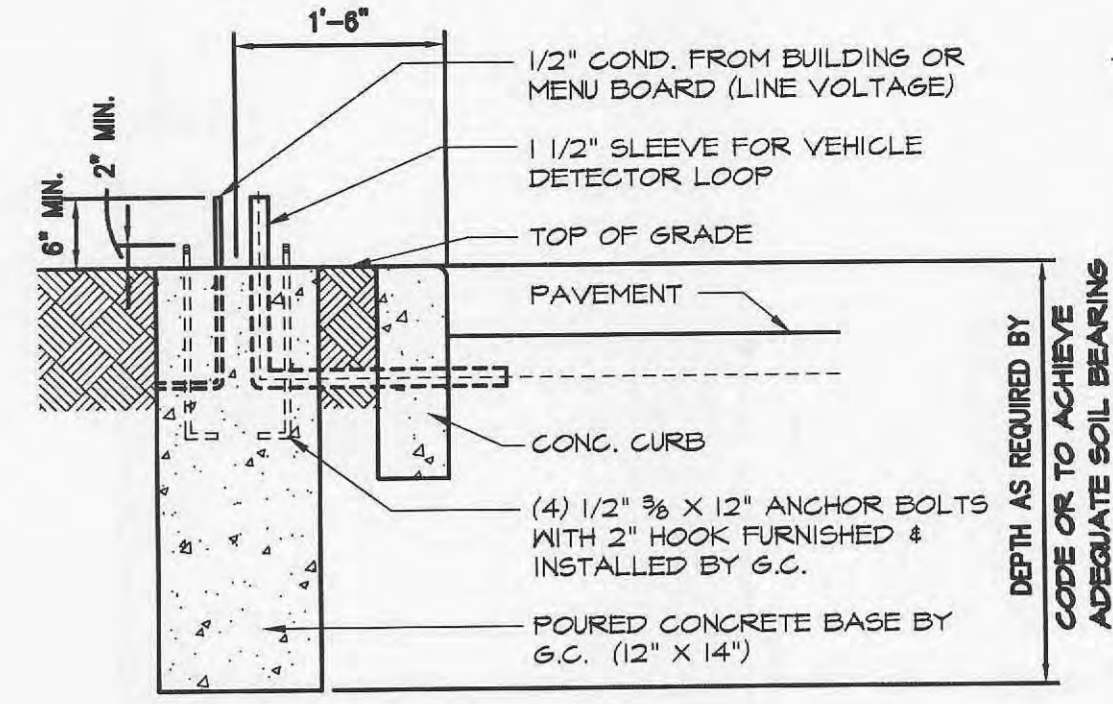
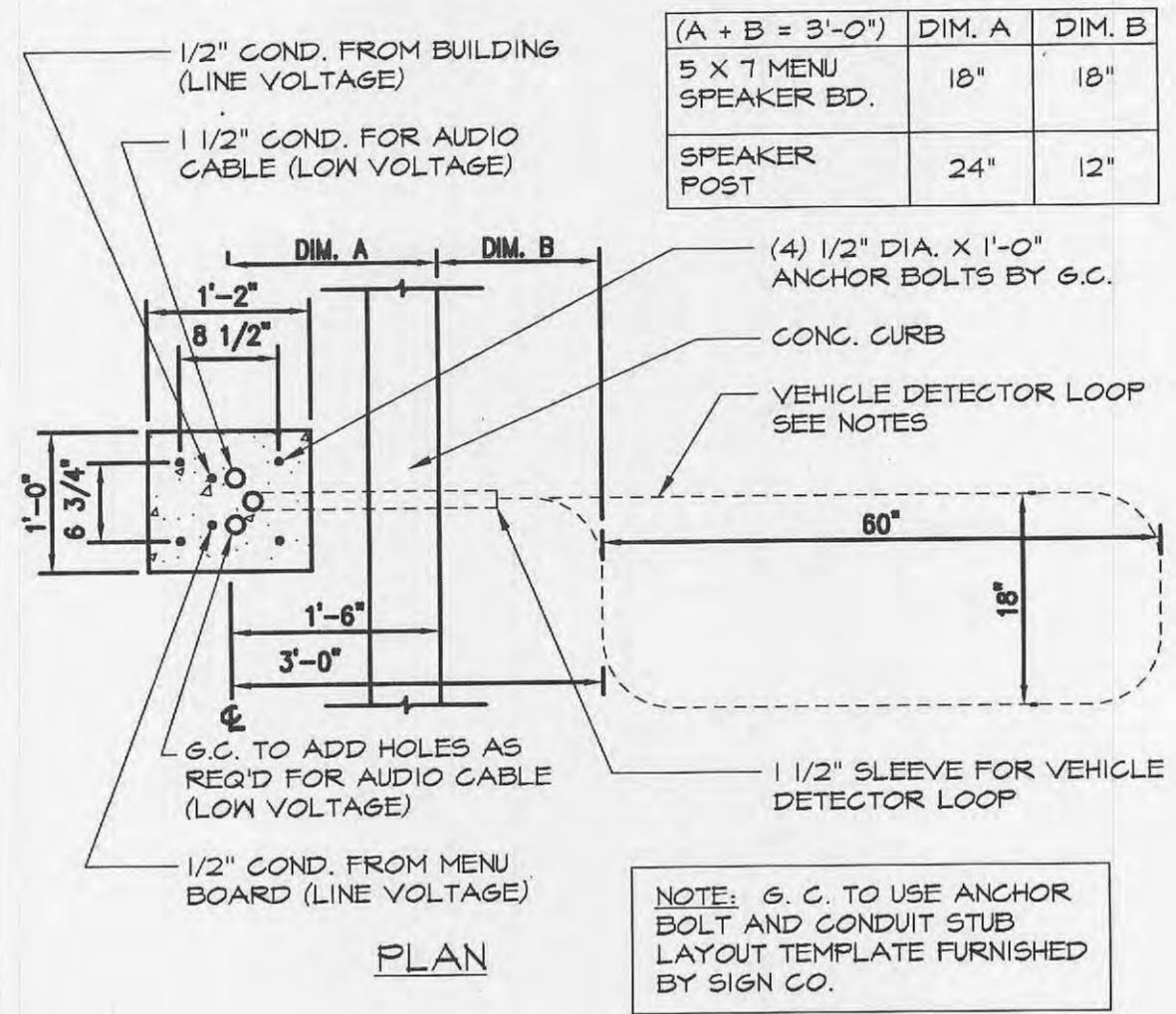
DETECTOR LOOP:

DISTRIBUTOR:
INTERSECTION DEVELOPMENT CORPORATION
1511 EAST ORANGETHORPE, SUITE A
FULLERTON, CALIFORNIA 92831
CONTACT: CHRISTINA 1-800-793-7812

MATERIAL:
POLYVINYL CHLORIDE TUBING 1/2" I.D. 100 PSI LOOP MADE FROM ONE LENGTH OF THIN FOURTEEN GAUGE STRANDED WIRE. LEAD-IN IS PRE-TWISTED AT FACTORY.

CONSTRUCTION:
FORMED WITH ONE CONTINUOUS LENGTH OF P.V.C. WITH NO SHARP CORNERS AS IN DRAWING BELOW. WIRE LOOPED, FORMED, AND PIGTAILED AS IN DRAWING BELOW.

NOTE:
DO NOT USE REINFORCEMENT RODS BELOW LOOP. KEEP ALL REBARS AND SCREENING AT LEAST TWO FEET AWAY FROM LOOP.



SECTION

NOTE: NOT REQUIRED FOR FACE-TO-FACE DRIVE-THRU SITES

SPEAKER POST FOOTING AND DETECTOR LOOP

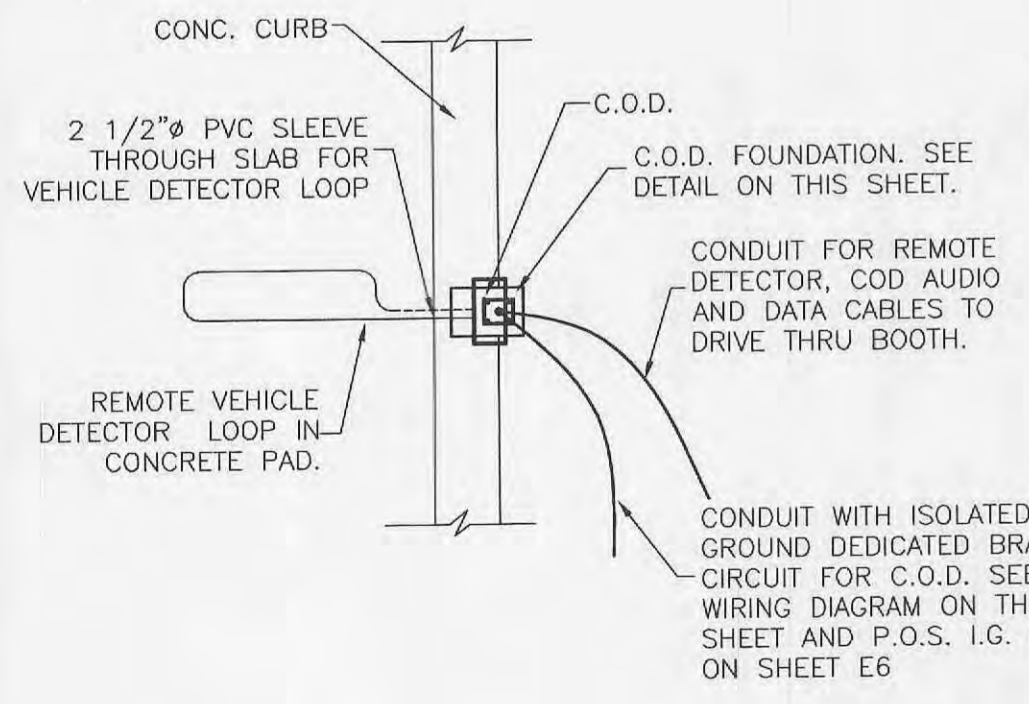
DETECTOR LOOP GENERAL NOTES

DISTRIBUTOR:
W.S. TRAFFIC
562-923-9600 (PHONE)
562-923-7555 (FAX)
MODEL #714 3'-0" X 8'-0"

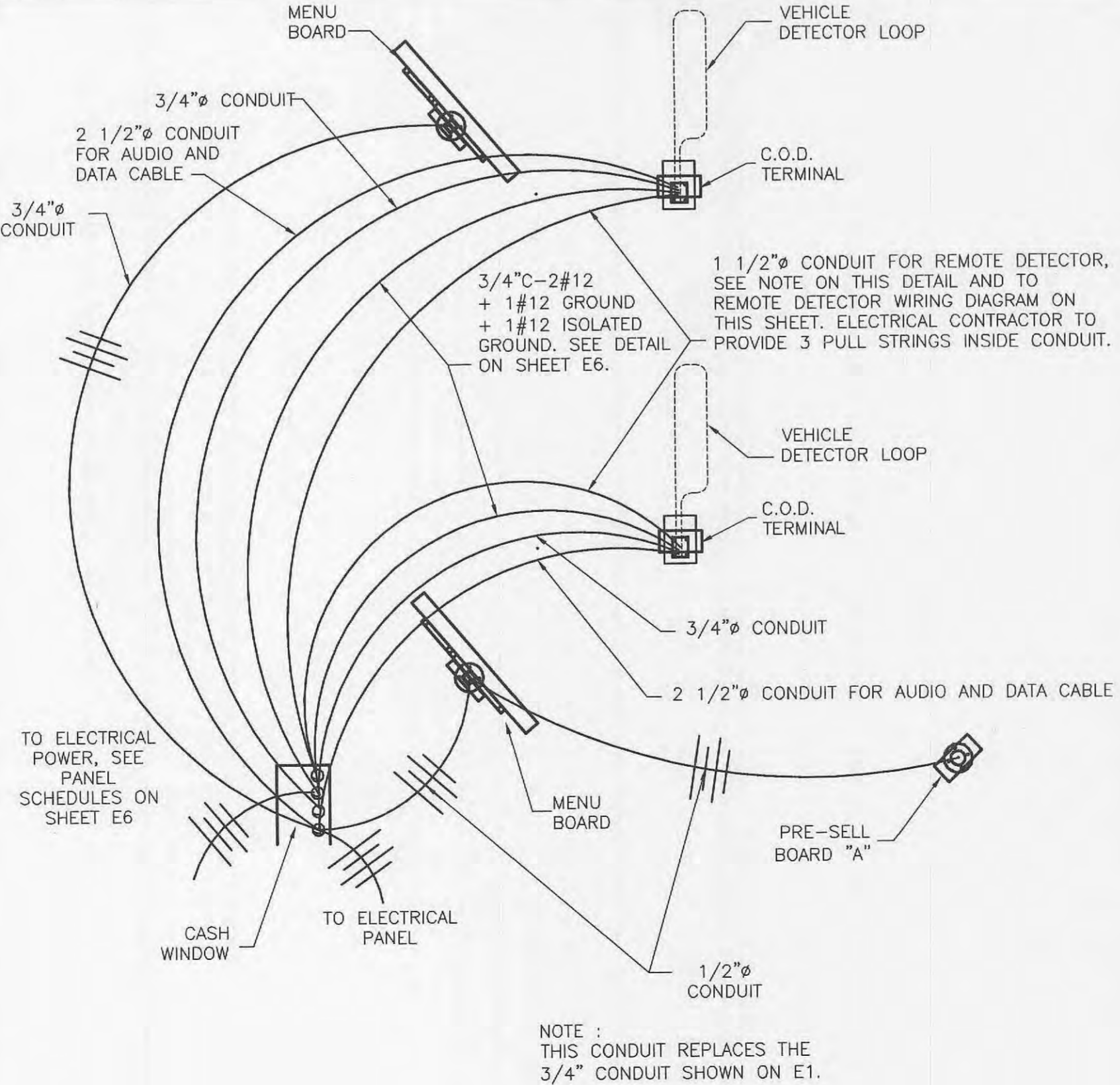
MATERIAL:
POLYVINYLCHLORIDE TUBING 1/2" I.D. 100 PSI LOOP MADE FROM ONE LENGTH OF THIN 14 GAUGE STRANDED WIRE. LEAD-IN IS PRE-TWISTED AT FACTORY.

CONSTRUCTION:
FORMED WITH ONE CONTINUOUS LENGTH OF PVC WITH NO SHARP CORNERS AS IN DRAWING ABOVE. WIRE LOOPED, FORMED & PIGTAILED AS ILLUSTRATED ON THIS SHEET.

NOTE:
DO NOT USE REINFORCEMENT RODS BELOW LOOP. KEEP ALL REBARS AND SCREENING AT LEAST 2'-0" AWAY FROM LOOP.

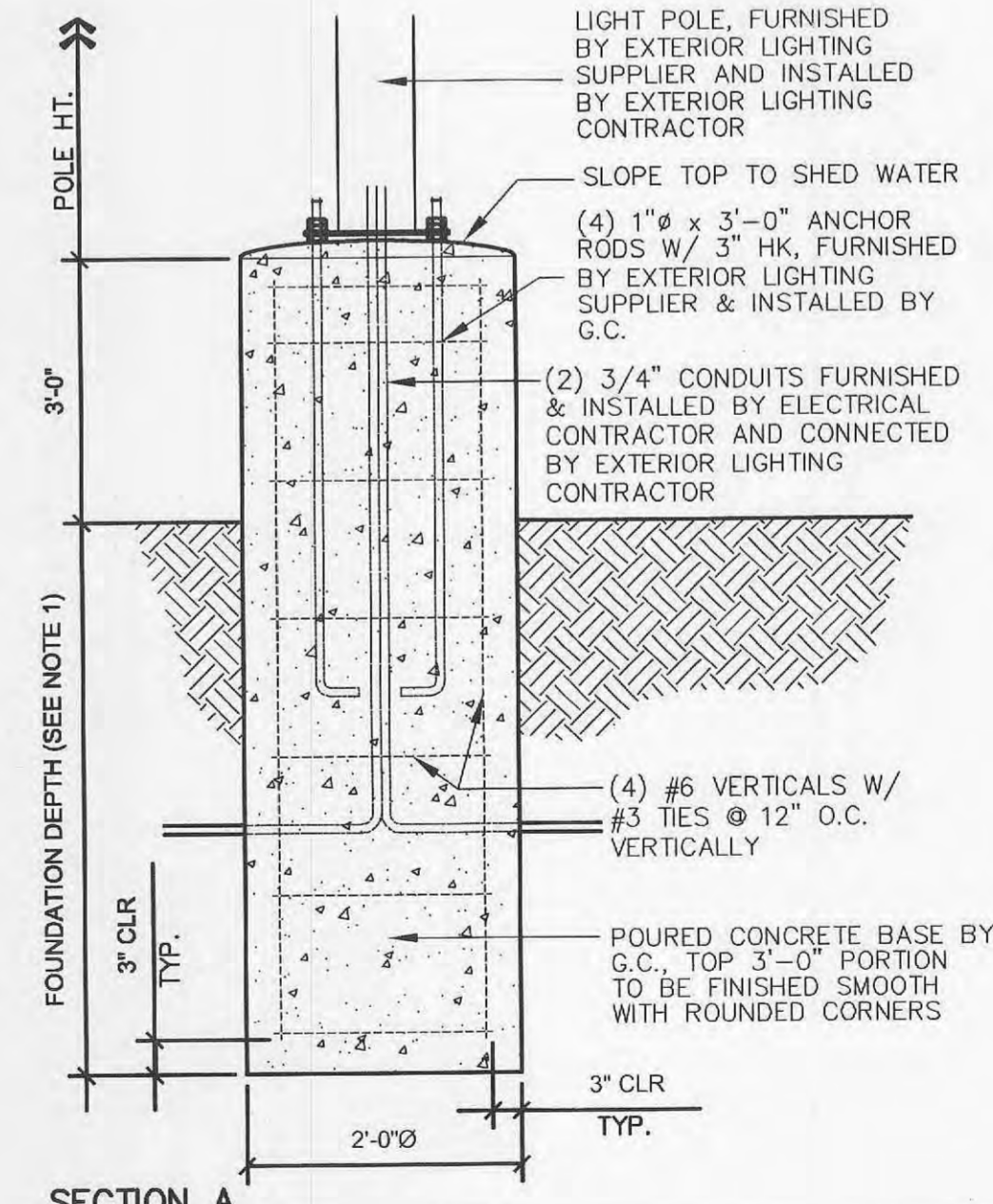


REMOTE VEHICLE DETECTOR WIRING DIAGRAM



NOTE: THIS CONDUIT REPLACES THE 3/4" CONDUIT SHOWN ON E1.

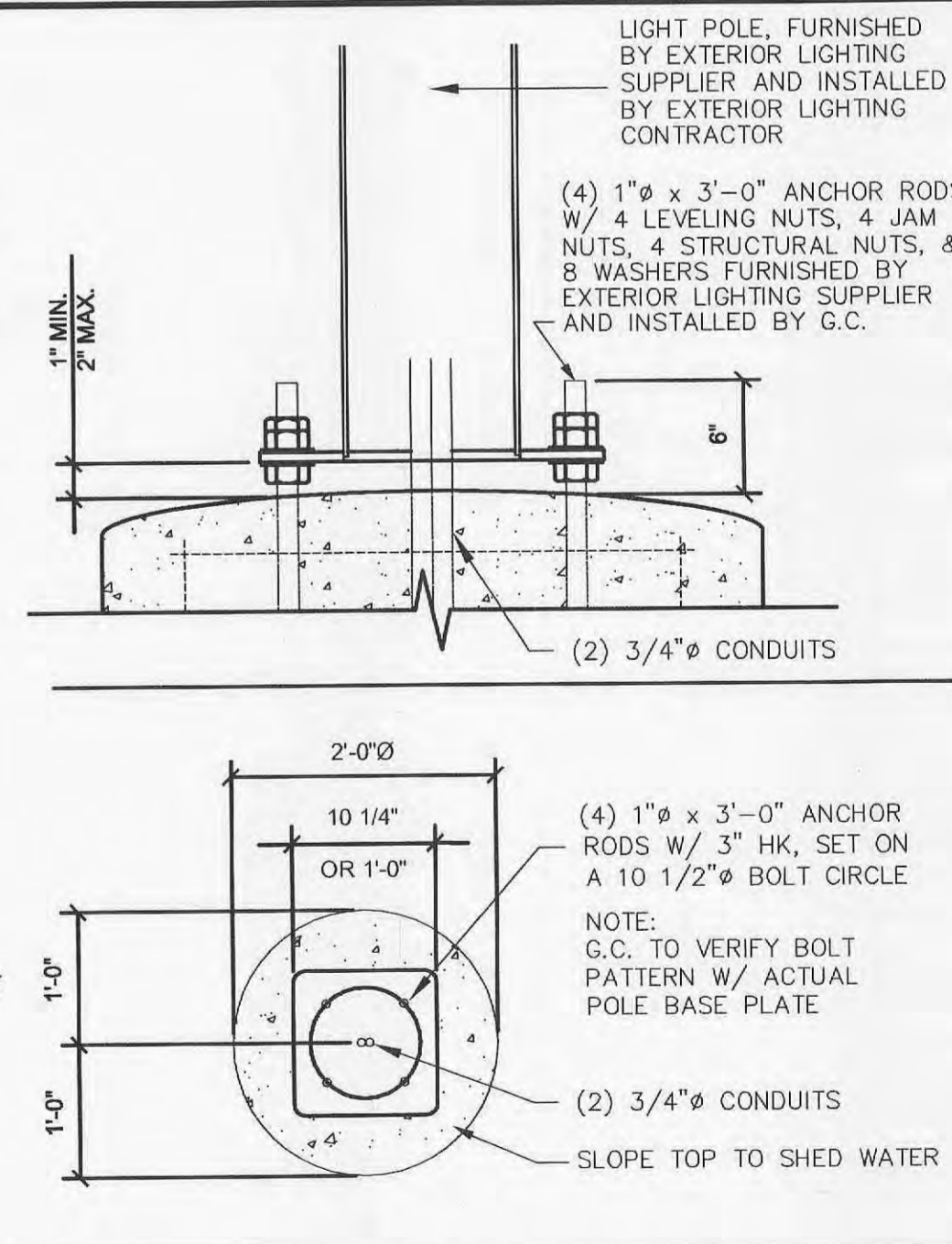
DRIVE THRU SITE WIRING DIAGRAM



SECTION A

- NOTES:
1. FOUNDATION DEPTH AS REQUIRED BY CODE OR TO ACHIEVE ADEQUATE SOIL BEARING CAPACITY AS DETERMINED IN A SOILS REPORT BY A LICENSED GEOTECHNICAL ENGINEER, 5'-0" DEPTH MINIMUM. IF THE EXISTING EARTH IS NON-VIRGIN FILL MATERIAL OR OTHERWISE UNSUITABLE CONSULT A LICENSED GEOTECHNICAL ENGINEER.
 2. PROVIDE CONCRETE MIXTURE IN ACCORDANCE WITH ACI301.
 - f_c = 3000psi
 - AIR ENTRAINMENT = 6% +/- 1%
 - WATER TO CEMENT RATIO = 0.45 MAX.

LIGHT POLE BASE



3. ANCHOR RODS SHALL BE SET IN ACCORDANCE WITH THE AISC "CODE OF STANDARD PRACTICE."
4. COMPLY WITH THE FOLLOWING ACI GUIDES:
 - ACI304 FOR CONCRETE PLACEMENT
 - ACI306.1 FOR COLD WEATHER PLACEMENT
 - ACI301 FOR HOT WEATHER PLACEMENT
 - ACI308.1 FOR CONCRETE CURING
5. COMPLY WITH CRSI'S "MANUAL OF STANDARD PRACTICE" AND ACI318 FOR PLACING REINFORCEMENT.

STATE OF MISSOURI
ROBERT E. POLK
REGISTERED PROFESSIONAL ENGINEER
NO. 000000000
DATE 10-26-11

McDonald's
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REV	DATE	DESCRIPTION	UNITS OF MEASURE
1			NONE
2			
3			
4			
5			

McDonald's CORPORATION SITE DETAILS

TITLE	MATERIAL / DESCRIPTION	CHECKED BY	DATE CHECKED
MENU BOARDS & DRIVE THRU DETAILS			

DRAWN BY	DATE DRAWN	APPROVED BY	DATE APPROVED
MLP	1-31-09		

D:\WORK\BUILDINGS\SD SHEETS
DRAWING ID: 1-31-09
PLOT: 1=1'

SHEET NO. **SD4**