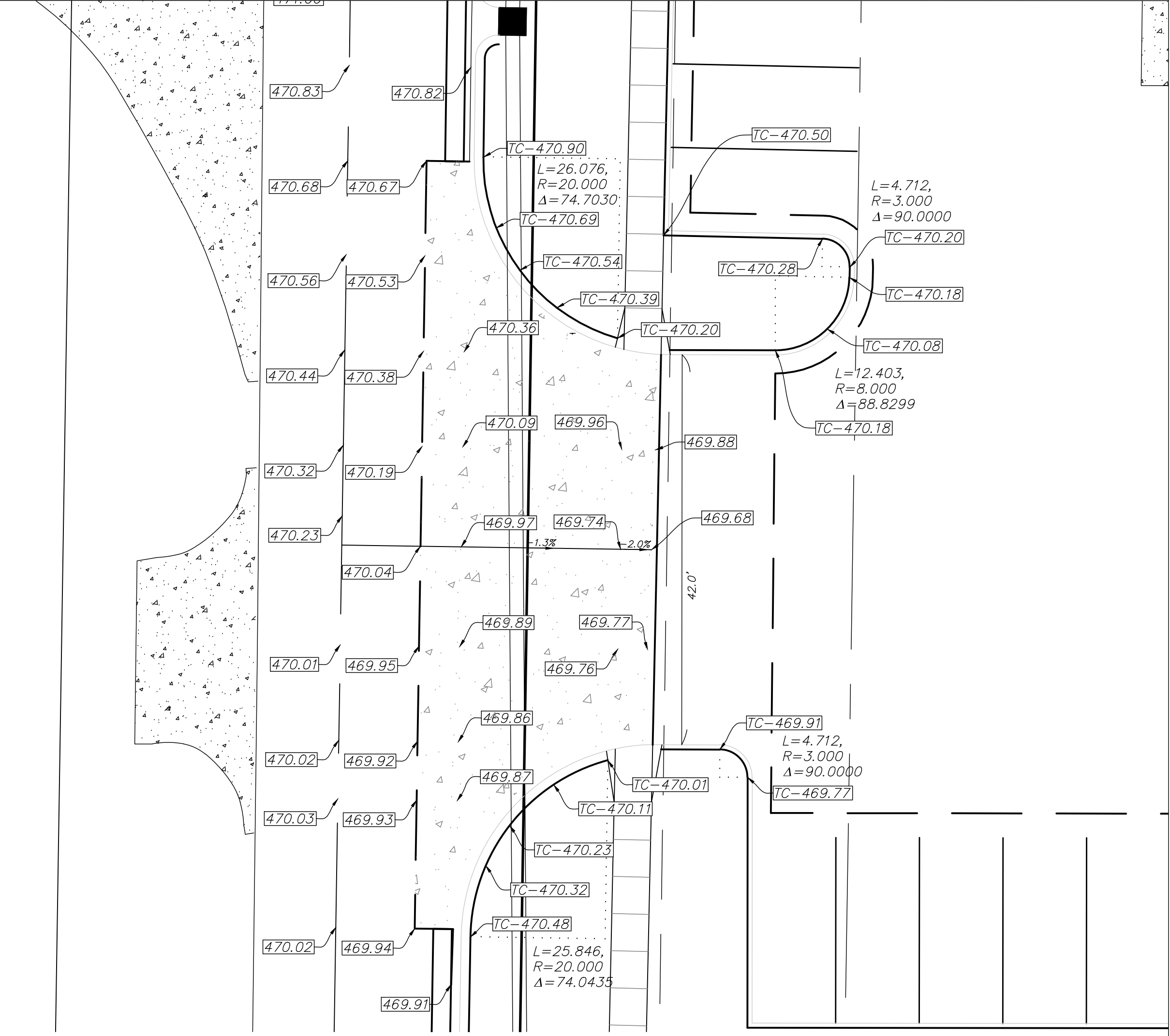
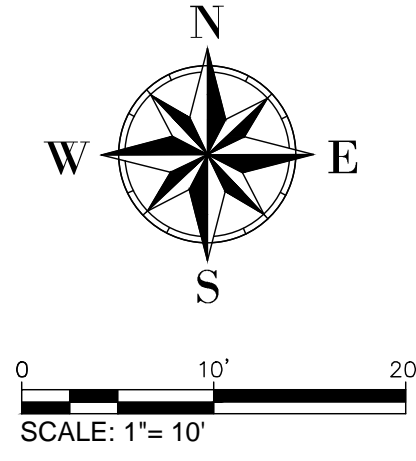
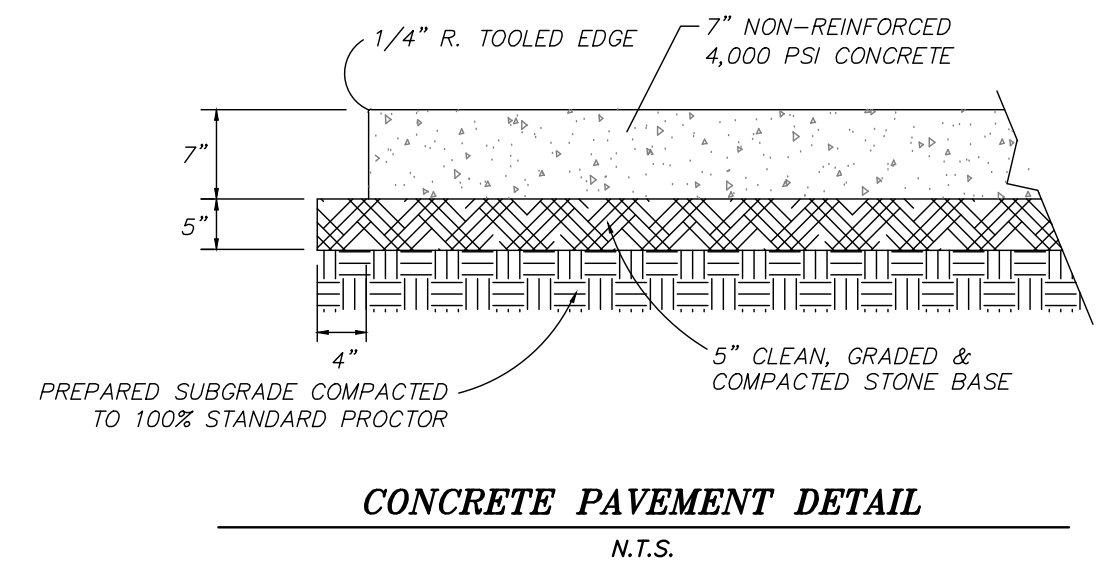


**UTILITIES NOTE**

UNDERGROUND FACILITIES, UTILITIES AND STRUCTURES HAVE BEEN PLOTTED FROM AVAILABLE INFORMATION AND THEREFORE, THEIR LOCATION MUST BE CONSIDERED APPROXIMATE ONLY. THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO HAVE ALL UTILITIES LOCATED IN THE FIELD PRIOR TO EXCAVATION OR CONSTRUCTION.

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- ROADWAY / ENTRANCE NOTES**
- PAVEMENT REMOVAL SHALL BE TO BE TO THE NEAREST JOINT.
  - SAWCUTS OF EXISTING PAVEMENT SHALL BE FULL DEPTH.

**616.8.10a (TA-10a) Lane Closure on Two-Lane Highways Without Edgelines Using Flaggers - MT**

SPEED (mph)	SIGN SPACING (ft.)		TAPER LENGTH (ft.)		OPTIONAL BUFFER LENGTH (ft.) (B)	CHANNELIZER SPACING (ft.)	
	Undivided (S)	Divided (S)	Shoulder (T1)	Lane (T2)		Tapers	Buffer/Work Areas
0-35	200	-	-	-	280	-	40
40-45	350	-	-	-	400	-	80
50-55	500	-	-	-	560	-	80
60-70	1000	-	-	-	840	-	120

1 Shoulder taper length based on 10 ft. (standard shoulder width) offset. 2 Lane taper length based on 12 ft. (standard lane width) offset.

A protective vehicle may be used while work is in progress. The protective vehicle may be equipped with a TMA and flashing arrow panel and positioned at least 150 ft. in advance of the work space.

If a flashing arrow panel is used, the caution mode may be displayed.

When a temporary road closure is needed, both directions may be stopped at the same time up to a maximum of 20 minutes.

Vehicle hazard warning signals shall not be used instead of the vehicle's rotating lights or strobe lights.

Where operational conditions warrant, channelizing devices may be eliminated.

For short duration operations, signs and channelizers may be reduced or eliminated.

For operations where workers are on foot and move with the operation, channelizers may be reduced or eliminated. If channelizers are eliminated, a protective vehicle should be used while work is in progress. The protective vehicle may be equipped with a TMA and/or flashing arrow panel and positioned at least 150 ft. in advance of the work space.

For operations where workers are on foot and move with the operation, spacing between flagger and FLAGGER AHEAD signs shall not exceed one mile.

Additional warning signs shall be erected at each intersection with another state highway within the work zone. Upon the discretion of the supervisor, additional warning signs may be erected at other intersections within the work zone.

For 3-2-1 Cone Procedure, refer to EPG 616.5 Flagging Control.

At night, flagger stations shall be illuminated.

For long-term operations, refer to EPG 616.6.2.2 Flags and Advance Warning Rail System.

If rumble strips are used, review EPG 616.8.7 RUMBLE STRIPS.

For work zone located in the vicinity of a railroad grade crossing, refer to EPG 616.8.46 (TA-46) Work in the Vicinity of a Grade Crossing.

SEE EPG 616.12 WORK ZONE SPEED LIMITS FOR SPEED LIMIT GUIDELINES.

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TA-10a 1/16

**PROJECT TITLE**  
IMPROVEMENT PLANS  
MIDWAY TRAILERS INC DBA  
MISSOURI GREAT DANE  
ENTRANCE DETAILS

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**ENGINEER'S AUTHENTICATION**  
KARL A. SCHENKE, P.E.  
PROFESSIONAL ENGINEER LICENSE 2003015039

**Developer / Owner Information**  
STEVE + PATTI JONES  
PO BOX 275  
Benton, Missouri 63796

P+Z No. 17-012024  
City No. 18-006464  
ENTRANCE DETAILS  
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City of O'Fallon Standard Subdivision Notes and Details - June 2010

\*\*\* 12/12/2018, Review Set, NOT APPROVED FOR CONSTRUCTION \*\*\*