

| CROSS SECTION | CONDITION | | TREATMENT | SIGN | | NOTES |
|---------------|---|-------------------------------|--|--------------|-------------------|--|
| | DIFFERENTIAL | TIME | | MAINLINE (1) | SIDE ROAD (2) | |
| | PAVEMENT EDGE DIFFERENTIAL > 2" TO ≤ 3" | NON-WORKING HOURS | WEDGE SLOPE TO 1V:1H OR FLATTER | | NO SIGNS REQUIRED | <p>(1) SIGNS SHALL BE SPACED AT APPROXIMATELY ONE MILE INTERVALS AND LOCATED WITHIN 150 FT. BEYOND ANY STATE ROADS. WHEN A SIGN PLACED AT THE ONE MILE INTERVAL FALLS WITHIN 1/2 MILE OF AN INTERSECTION, THE SIGN PLACED AT THE 1/2 MILE INTERVAL MAY BE OMITTED. WHEN LOW SHOULDER/SOULDER DROP-OFF SIGNS WITH UNEVEN LANES ARE BOTH SPECIFIED, ALTERNATING SIGN MESSAGES SHALL BE USED AT 1 MILE SPACINGS.</p> <p>(2) ON SIDE ROADS WITH POSTED SPEED OF 45 MPH OR GREATER, SIGNS SHALL BE PLACED 150 FT. IN ADVANCE OF INTERSECTION WITH MAINLINE.</p> <p>(3) SIGNS SHALL BE LOCATED ON THE SIDE OF THE ROADWAY WHERE THE PAVEMENT EDGE DIFFERENTIAL EXISTS. ON TWO-LANE UNDIVIDED HIGHWAYS, BACK-TO-BACK SIGNS SHALL BE PROVIDED ON THE SIDE OF THE ROADWAY, WHERE THE PAVEMENT EDGE DIFFERENTIAL EXISTS. "BACK TO BACK" SIGNS SHALL BE SEPARATED BY 7-10 FEET.</p> <p>(4) SIGNS TO REMAIN VISIBLE UNTIL SHOULDER SHAPING IS COMPLETE.</p> <p>(5) SIGNS SHALL BE LOCATED ON RIGHT SIDE OF NON-DIVIDED HIGHWAYS AND ON BOTH SIDES OF DIVIDED HIGHWAYS WHERE A LANE LINE DIFFERENTIAL EXISTS.</p> <p>(6) WHEN THE SHOULDER DROP-OFF SIGNS ARE IN PLACE FOR GREATER THAN THREE DAYS, THE SHOULDER DROP-OFF PLAQUE SHOULD BE USED IN ADDITION WITH THE SHOULDER DROP-OFF SIGN.</p> <p>LEGEND: - RIGID PAVEMENT - FLEXIBLE PAVEMENT - ALL PAVEMENT TYPES</p> <p>GENERAL NOTES: SIGNS SHALL BE VISIBLE TO TRAFFIC ONLY WHEN AND WHERE CONDITIONS EXIST. FOR ADDITIONAL SIGN SPACING AND DETAILS SEE STANDARD PLAN 620.10.</p> |
| | | WORKING HOURS | NO EDGE TREATMENT REQUIRED | | NO SIGNS REQUIRED | |
| | PAVEMENT EDGE DIFFERENTIAL > 2" TO ≤ 3" | NON-WORKING HOURS | WEDGE SLOPE TO 1V:1H OR FLATTER | | NO SIGNS REQUIRED | |
| | | WORKING HOURS | NO EDGE TREATMENT REQUIRED | | NO SIGNS REQUIRED | |
| | PAVEMENT EDGE DIFFERENTIAL > 3" | NON-WORKING HOURS | WEDGE SLOPE TO 1V:3H OR FLATTER | | | |
| | | WORKING HOURS | DELINEATE DIFFERENTIAL WITH CHANNELIZERS | | W08-1 (3) | |
| | LANE LINE DIFFERENTIAL ≤ 2" | NON-WORKING AND WORKING HOURS | NO EDGE TREATMENT REQUIRED | | NO SIGNS REQUIRED | |
| | | NON-WORKING AND WORKING HOURS | WEDGE SLOPE TO 1V:3H OR FLATTER | | NO SIGNS REQUIRED | |
| | LANE LINE DIFFERENTIAL > 2" | NON-WORKING AND WORKING HOURS | WEDGE SLOPE TO 1V:3H OR FLATTER | | | |
| | | NON-WORKING AND WORKING HOURS | DELINEATE DIFFERENTIAL WITH CHANNELIZERS | | W08-1 (5) | |

1 PAVEMENT EDGE TREATMENT DETAIL
N.T.S.

616.8.6 (TA-6) SHOULDER WORK WITH MINOR ENCROACHMENT ON DIVIDED AND UNDIVIDED HIGHWAYS - DE/CM

| SPEED Normal Posted (mph) | SIGN SPACING (ft.) | | TAPER LENGTH (ft.) | | OPTIONAL BUFFER LENGTH (ft.) (B) | CHANNELIZER SPACING (ft.) Tapers | Buffer/Work Areas |
|---------------------------|--------------------|-------------|--------------------|-----------|----------------------------------|----------------------------------|-------------------|
| | Undivided (S) | Divided (S) | Shoulder (T1) | Lane (T2) | | | |
| 0-35 | 200 | 200 | 70 | 245 | 280 | 35 | 40 |
| 40-45 | 350 | 500 | 150 | 540 | 400 | 40 | 80 |
| 50-55 | 500 | 1000 | 185 | 660 | 560 | 50 | 80 |
| 60-70 | 1000 | | 235 | 840 | 840 | 60 | 120 |

1 Shoulder taper length based on 10 ft. (standard shoulder width) offset. 2 Lane taper length based on 12 ft. (standard lane width) offset.

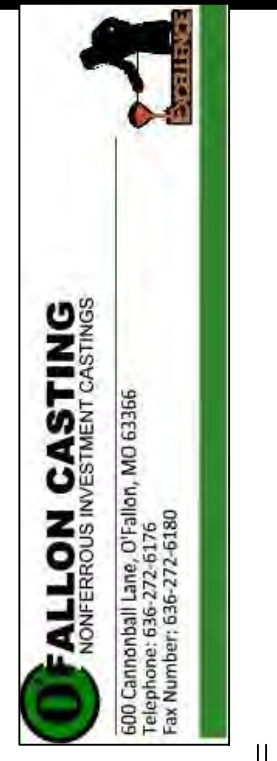
NOTES:
 PROVIDE SIGNS ON LEFT AND RIGHT SIDES OF DIVIDED HIGHWAYS
 ROAD WORK AHEAD SIGN NOT NEEDED IF SHOULDER WORK IS LOCATED WITHIN THE LIMITS OF AN ACTIVITY AREA WHERE ANOTHER ROAD WORK AHEAD SIGN IS ALREADY USED.
 SEE EPG 616.12 WORK ZONE SPEED LIMITS FOR SPEED LIMIT GUIDELINES.
 (3) NEXT XX MILES SIGN NOT REQUIRED FOR NARROW LANE SECTIONS LESS THAN ONE MILE.
 THE PROTECTIVE VEHICLE MAY BE OMITTED IF A TAPER AND CHANNELIZING DEVICES ARE USED.
 VEHICLE - MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSCURED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE - MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM VIEW WHEN WORK IS NOT IN PROGRESS.
 VEHICLE HAZARD WARNING SIGNALS SHALL NOT BE USED INSTEAD OF VEHICLE'S ROTATING LIGHTS OR STROBE LIGHTS.
 SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-DENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

2 TRAFFIC CONTROL DETAIL
N.T.S.

MISSOURI HIGHWAYS AND TRANSPORTATION COMMISSION
 105 WEST CAPITOL JEFFERSON CITY, MO 65102
 1-888-ASK-MDOT (1-888-275-6636)

PAVEMENT EDGE TREATMENT

DATE EFFECTIVE: 10/01/2015
 DATE PREPARED: 8/24/2016
 SHEET NO. 1 OF 1



ADDITION & RENOVATION FOR
O'FALLON CASTING
 600 CANNONBALL LANE OFALLON, MISSOURI 63366
 CONSTRUCTION DETAILS

REVISIONS

| NO. | DATE |
|-----|------------|
| 3 | 08/10/2016 |
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| | |

ISSUE DATE: MAY 26, 2016
 PROJECT NO.: 15-187
 DRAWN BY: GAS
 CHECKED BY: TBS

SEAL

The Professional Engineer's responsibility for the design and construction of the project is limited to the work shown on this sheet. The Engineer does not assume any liability for the design and construction of the project unless the Engineer is specifically named in the contract documents.

SHEET
C8.04