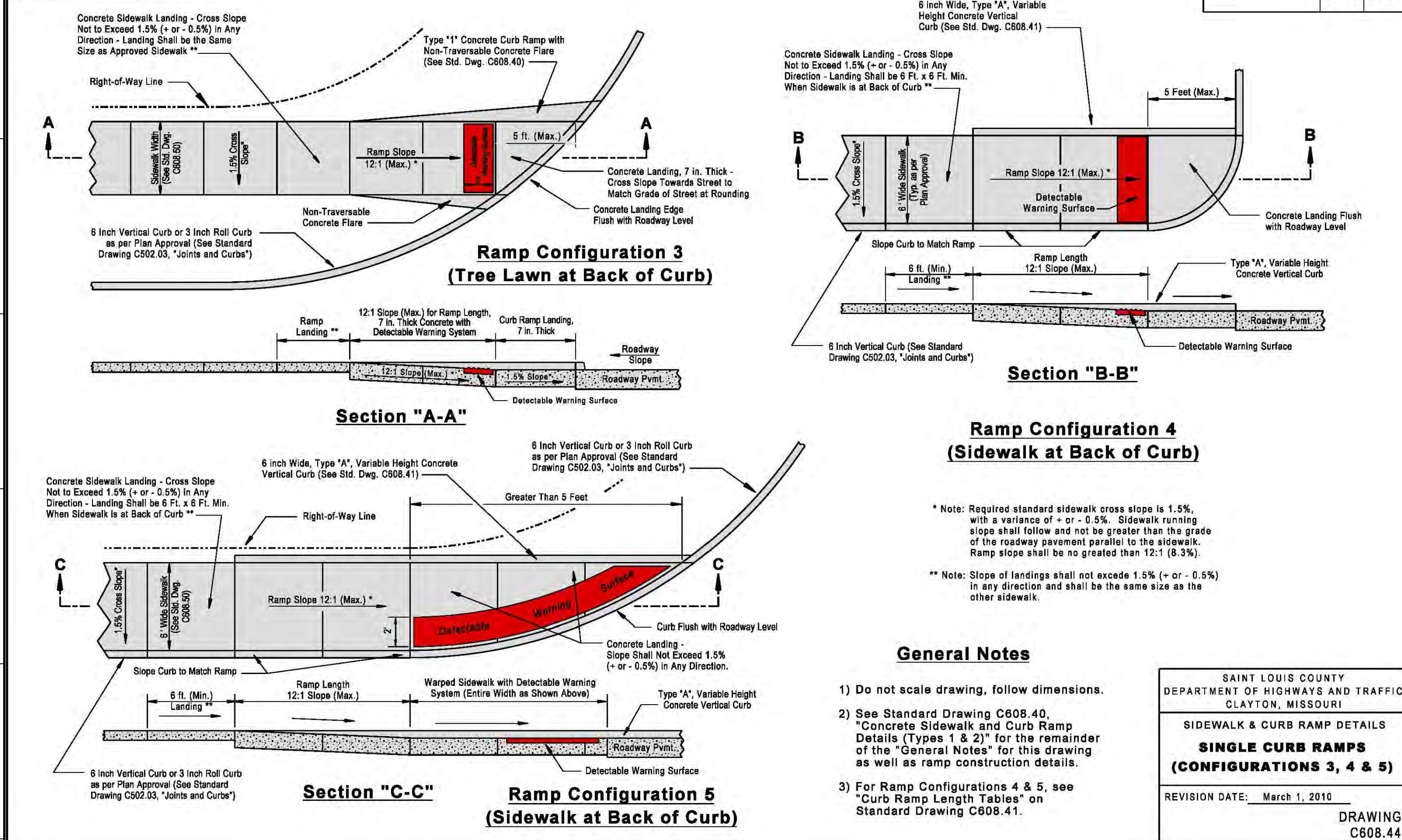


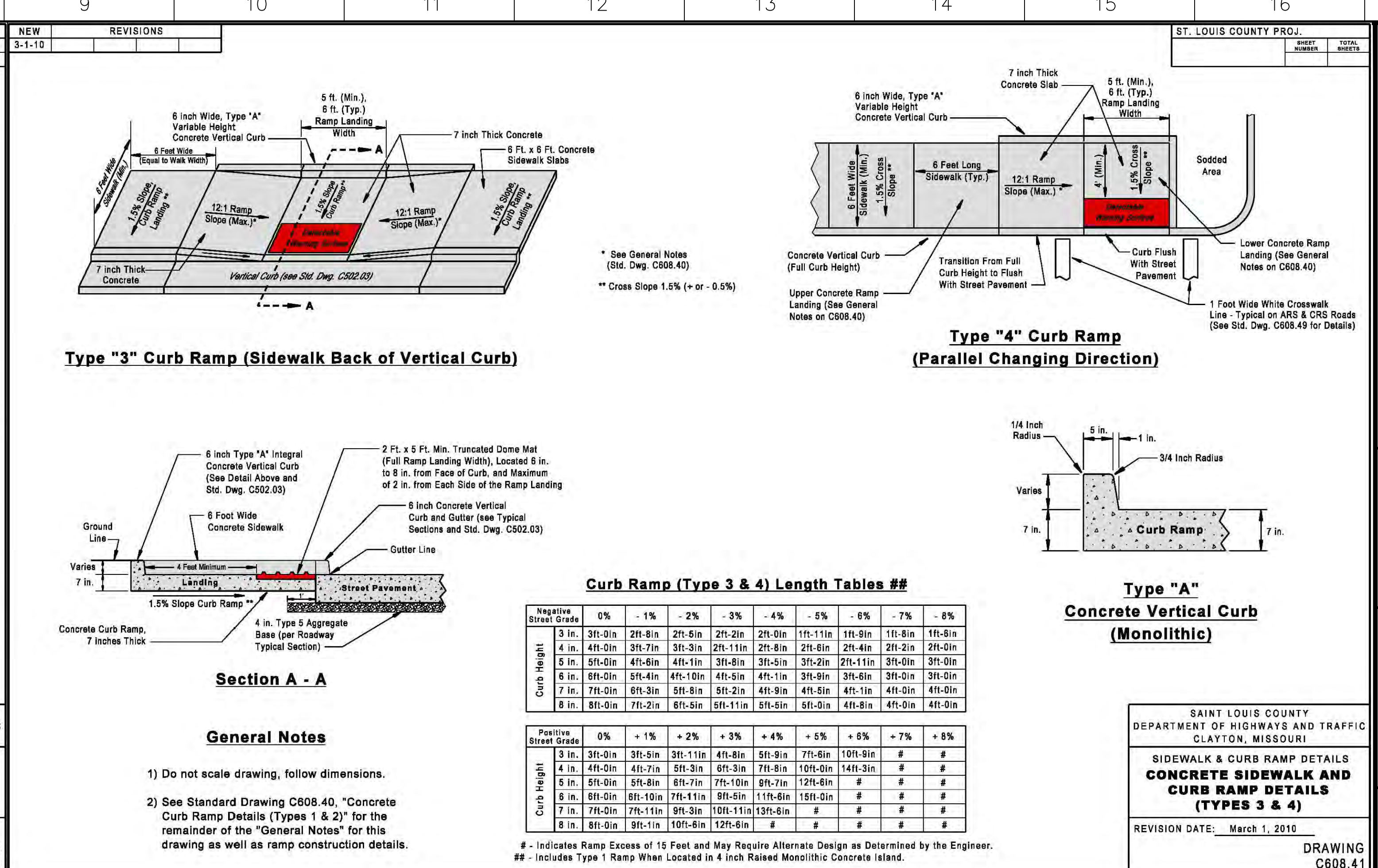
NEW	REVISIONS
3-1-10	7-1-10



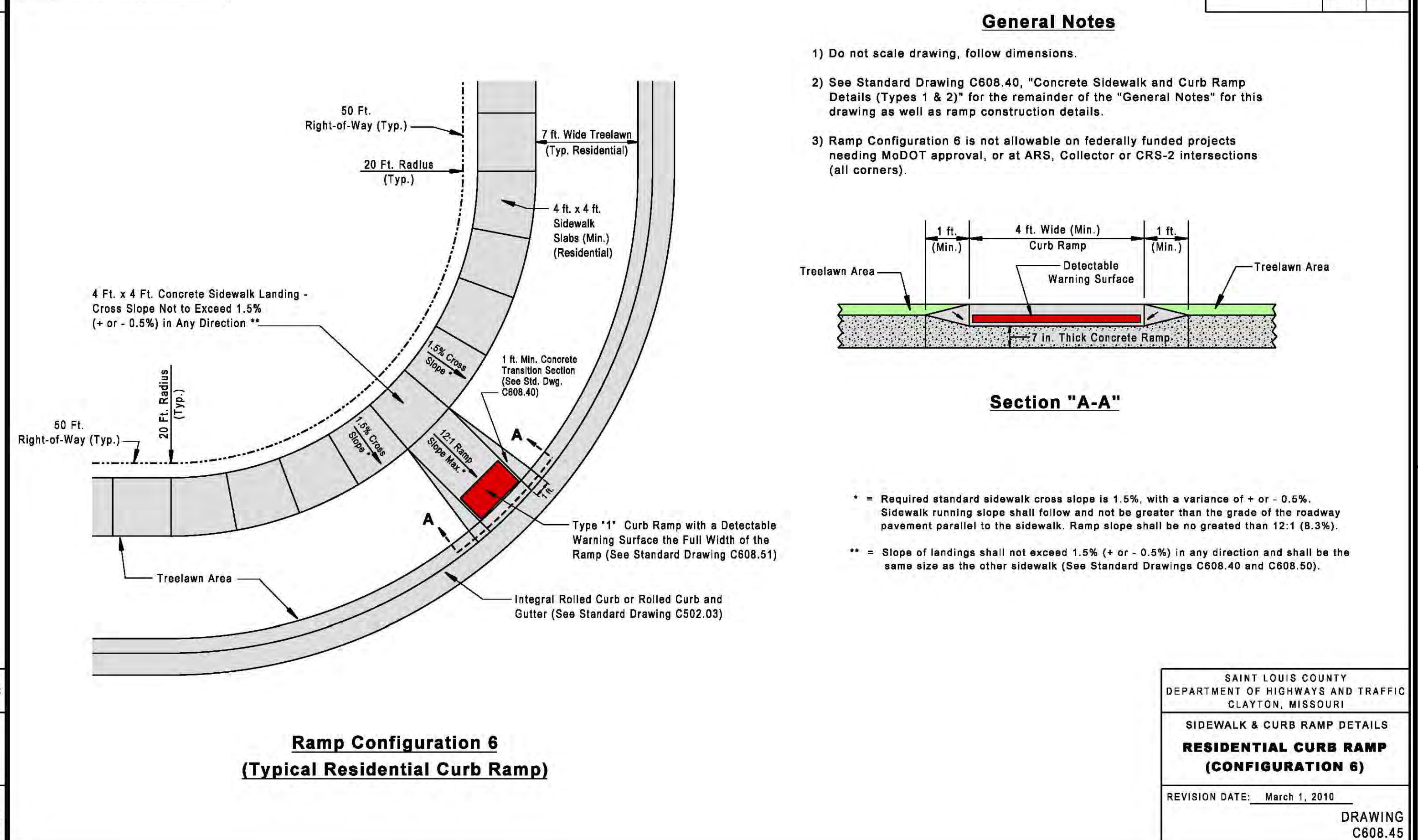
NEW	REVISIONS
3-1-10	7-1-10



NEW	REVISIONS
3-1-10	7-1-10



NEW	REVISIONS
3-1-10	7-1-10



NEW	REVISIONS
3-1-10	7-1-10



NEW	REVISIONS
3-1-10	7-1-10

**General Notes (Cont.)**

- Where a curb ramp meets the pavement, a bullnose will not be permitted. Transition from the sidewalk to the gutter to the roadway surface must be flush. If a monolithic concrete curb is constructed, strike a dummy joint across the bottom of the ramp and the concrete curb.
- Side flares of curb ramps, in the path of pedestrian travel (traversable), shall not exceed a slope of 10H:1V. Side flares outside the pedestrian path (non-traversable) may be 2H:1V slope or vertical.
- For sidewalk locations on cut-to-slopes, refer to Pavement Construction Details (Std. Dwg. C502.03). For pavement longitudinal and transverse joints and tie bar requirements and dimensions, refer to the Pavement Construction Details for "Joints and Curbs" (Std. Dwg. C502.03). For roadway cross slopes, pavement types, and thickness, refer to the necessary Standard Typing Section.
- Landings shall be constructed at the top and/or bottom of a curb ramp whenever a turning movement or access to pedestrian push button is required. Landings located in the roadway must be completely contained within the crosswalk.
- Detectable Warning Surface (DWS) shall consist of a surface of truncated domes aligned in a square or radial grid pattern. The pattern shall be aligned with the edge of pavement in the direction of travel. The DWS shall extend 24 inches in the direction of travel and span the full width of the curb ramp (exclusive of flares), landing or blended transition. The DWS shall contrast visually with the ramp, landing, raised island and cut-through island surfaces, and shall be dark-on-light (or light-on-dark with the approval of the engineer) (see Std. Dwg. C308.5). Truncated domes shall be aligned parallel to the running slope of a ramp, in a square grid, when installed in the direction of pedestrian travel, to permit wheels to roll between the domes. When installed on a landing or blend transition, DWS shall be aligned perpendicular or radial to the break between the ramp, landing or blended transition and the street. DWS shall be installed 6 in. to 8 in. from the face of curb (see Detail "A" on this drawing). DWS to concrete cut-through islands, shall be aligned with the face of the curb of the cut-through island (see Std. Dwg. C502.03). The Detectable Warning Surface (DWS) or Truncated Dome Mats, shall be consistent throughout the project and are required to be installed on all curb ramps at public and private street intersections, signalized entrances; and at channelized or raised islands where a curb ramp or cut-through is provided. DWS may be omitted on curb ramps adjacent to non-signalized private or residential entrances. No direct payment will be made for detectable warning surfaces.
- The DWS type shall be "Cast-in-Place with Screw Anchoring" (both new and retrofit application) or "Surface Applied with Adhesive" (retrofit only), with the approved manufacturer listed on the "MODOT Qualified Lists, Qualified Truncated Domes, FS-1067 Table 1" (also see Std. Dwg. C608.51, General Note 4). The "MODOT Qualified Lists" are located on Internet at: [www.modot.com/central/central/traffic/7802183.dwg](http://www.modot.com/central/central/traffic/7802183.dwg)
- Detectable Warnings (truncated domes) shall be installed per manufacturer's recommendations and instructions (see Std. Dwg. C608.51). Truncated domes shall span the full width of the ramp, landing or cut-through island. Stamped concrete truncated domes or grooves will not be accepted.
- The running grade of a sidewalk shall not exceed 5.0% unless it is matching the grade of the adjacent roadway.
- Storm water inlets, signs, posts, manhole covers, pull boxes and other access lids should be avoided within the sidewalk. If such a location is necessary, the feature must meet ADA standards. Mailbox placement should also be avoided in the sidewalk. If it is necessary, use the standards shown on Standard Drawing C608.55, "Mailbox Placement Standards". Utility poles will not be allowed in the sidewalk. An accessible route must have minimum 3 foot (36 inch) clear width (ADAAG).
- All sidewalk sections, bus stop pads, and landings at the top of curb ramps, shall be paid for as "Concrete Sidewalk". Curb ramps and landings at the bottom of curb ramps, shall be paid for as "Concrete Curb Ramp". No direct payment will be made for variable height curbs, concrete flares, or saw cut when constructed as part of a landing or curb ramp. No direct payment will be made for pre-mixed expansion joint filler with joint sealer.

**General Notes**

- Do not scale drawing, follow dimensions.
- See Standard Drawing C608.40, "Concrete Sidewalk and Curb Ramp Details (Types 1 & 2)" for the remainder of the "General Notes" for this drawing as well as ramp construction details.

**General Notes**

- Do not scale drawing, follow dimensions.
- See Standard Drawing C608.40, "Concrete Sidewalk and Curb Ramp Details (Types 1 & 2)" for the remainder of the "General Notes" for this drawing as well as ramp construction details.
- For Ramp Configurations 4 & 5, see "Curb Ramp Length Tables" on Standard Drawing C608.41.

SAINT LOUIS COUNTY	DEPARTMENT OF HIGHWAYS AND TRAFFIC	CLAYTON, MISSOURI
SIDEWALK & CURB RAMP DETAILS		
CONCRETE SIDEWALK AND CURB RAMP DETAILS (TYPES 1 & 2)		
REVISION DATE:	July 1, 2010	
DRAWING	C608.40	

SAINT LOUIS COUNTY	DEPARTMENT OF HIGHWAYS AND TRAFFIC	CLAYTON, MISSOURI
SIDEWALK & CURB RAMP DETAILS		
SINGLE CURB RAMP DETAILS (CONFIGURATIONS 3, 4 & 5)		
REVISION DATE:	March 1, 2010	
DRAWING	C608.44	

SAINT LOUIS COUNTY	DEPARTMENT OF HIGHWAYS AND TRAFFIC	CLAYTON, MISSOURI
SIDEWALK & CURB RAMP DETAILS		
RESIDENTIAL CURB RAMP (CONFIGURATION 6)		
REVISION DATE:	March 1, 2010	
DRAWING	C608.45	

SAINT LOUIS COUNTY	DEPARTMENT OF HIGHWAYS AND TRAFFIC	CLAYTON, MISSOURI
SIDEWALK & CURB RAMP DETAILS		
CONCRETE SIDEWALK AND CURB RAMP DETAILS (TYPES 3 & 4)		
REVISION DATE:	March 1, 2010	
DRAWING	C608.41	

SAINT LOUIS COUNTY	DEPARTMENT OF HIGHWAYS AND TRAFFIC	CLAYTON, MISSOURI
SIDEWALK & CURB RAMP DETAILS		
CONCRETE SIDEWALK AND CURB RAMP DETAILS (TYPES 3 & 4)		
REVISION DATE:	March 1, 2010	
DRAWING	C608.41	

SAINT LOUIS COUNTY	DEPARTMENT OF HIGHWAYS AND TRAFFIC	CLAYTON, MISSOURI
SIDEWALK & CURB RAMP DETAILS		
RESIDENTIAL CURB RAMP (CONFIGURATION 6)		
REVISION DATE:	March 1, 2010	
DRAWING	C608.45	

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**EOCHRAN**  
Civil Engineering  
Land Surveying  
Architecture  
Site Development  
General Consulting  
Master Planning

Missouri State Certificate of Authority Numbers:  
Survey: 000380  
Engineering: 001655  
Architecture: 200214240

Three working days prior to the start of any excavation, call 1-800-463-8172 for utility location information.

All OSHA rules & regulations construction required by these plans and be strictly followed (i.e. trenching, blasting, etc.)

STATE OF MISSOURI  
1/31/21  
ELLIOTT R. REED  
Professional Engineer  
NUMBER  
PE-200602345

THE STREETS OF CALEDONIA  
PHASE II RESIDENTIAL  
O'FALLON, MISSOURI

DATE: JMM  
APPROVED: ERR  
DATE: JAN. 13, 2021  
SCALE: NO SCALE  
PROJ. NO: 17-6821  
DWG. NO: C14.03

OWNER/DEVELOPER:  
DD LAND DEVELOPMENT, LLC  
10407 BAUR BLVD, SUITE B  
ST. LOUIS, MO 63132

Drawing name: F:\17-6821 - 260 Acre Tract along Hwy 40, O'Fallon, MO\AutoCad Drawings\6821 - Plan Sheets\Construction Drawings\PH2 Residential Plans\C14.03 Detail Sheet Phase 2.dwg Tab: C14.03 Plotted on: Jan 13, 2021 3:48pm Plotted by: jmeyer