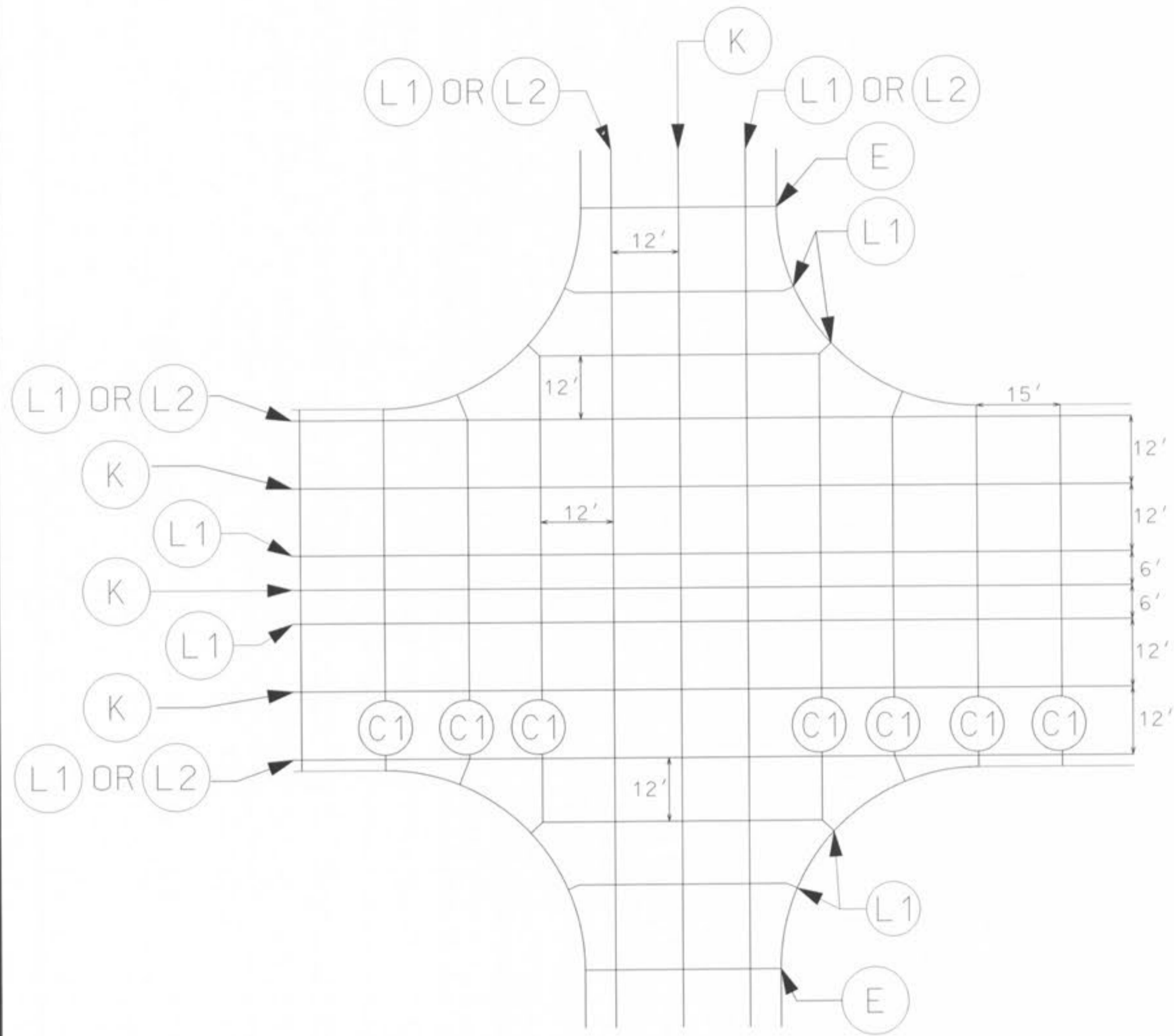


NOT TO SCALE



JOINT DETAILS

GENERAL NOTES:

THE CONTRACTOR SHALL DETERMINE THE PAVING WIDTH. (K) OR (L2) JOINTS SHALL BE USED BETWEEN ADJOINING SECTIONS OF PAVEMENT. (L1) JOINTS SHALL BE USED WITHIN THE PAVING WIDTH.

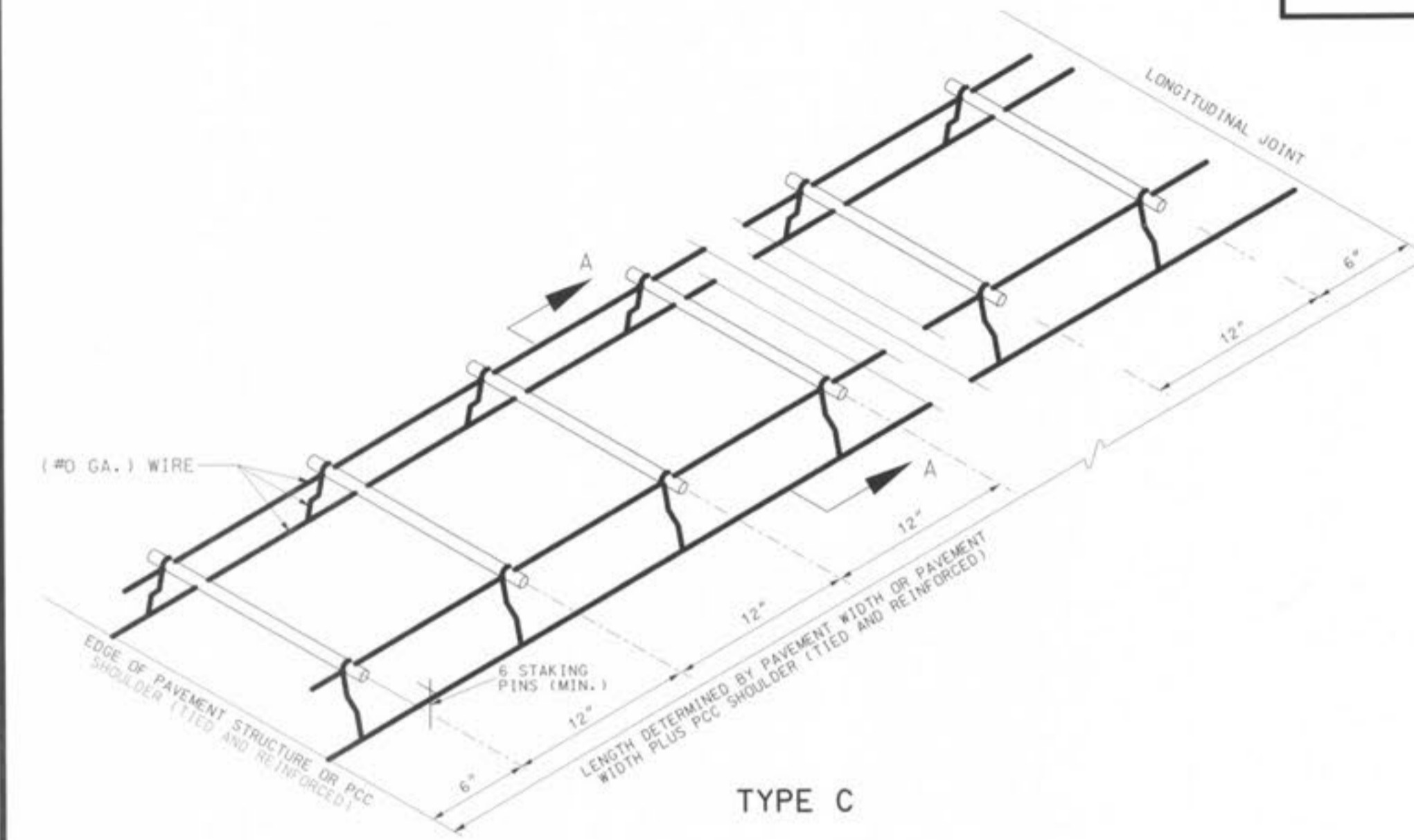
FOR JOINT DETAILS SEE JOINT DETAIL SHEETS 2 AND 3.

JOINT SPACING SHOWN IS MAXIMUM AND IS TO BE REDUCED TO AVOID CONFLICT WITH ABUTTING STRUCTURES. JOINTS IN MULTI-LANE PAVEMENT ARE TO BE CONTINUOUS.

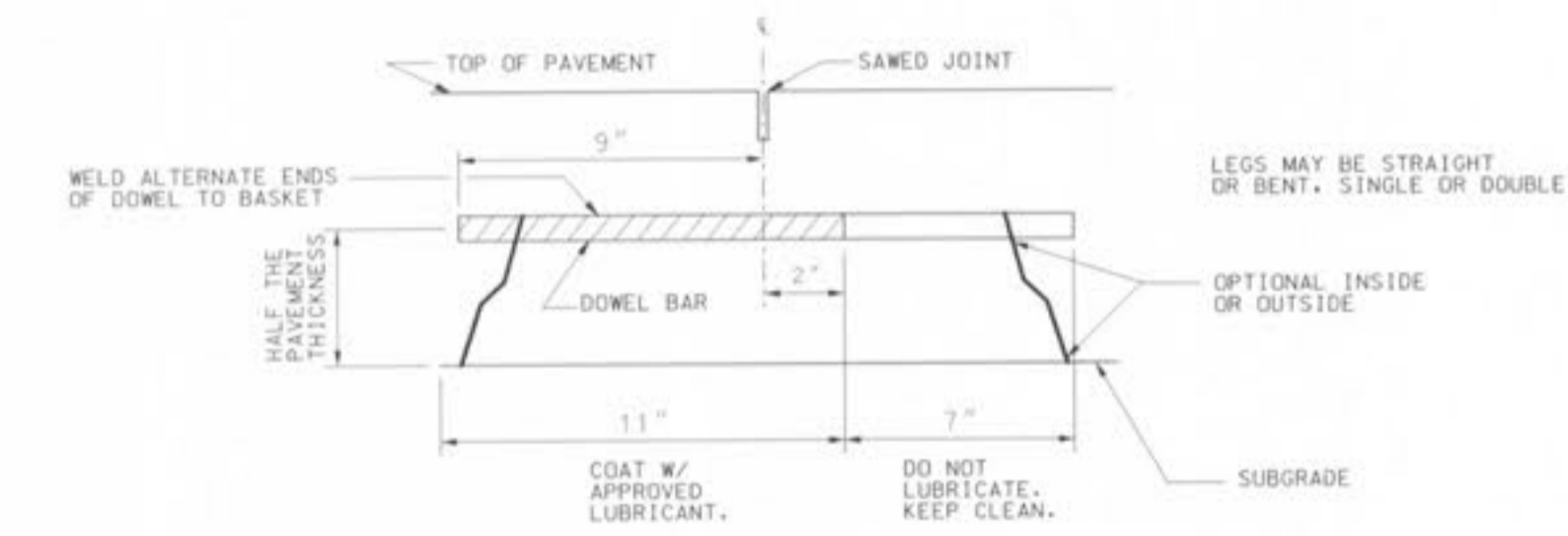
CONTRACTOR SHALL LAYOUT JOINT DETAIL PRIOR TO PERMANENT PLACEMENT FOR CITY OF O'FALLON FIELD ENGINEER APPROVAL.

GBA GEORGE BUTLER ASSOCIATES, INC. Engineers • Architects		DATE: JULY 2007
TOM GINNEVER ROAD IMPROVEMENTS PHASE I O'FALLON, MISSOURI		DESIGN BY: DTS DRAWN BY: CDF PROJECT NO.: 10716
SHEET NO.: 56	TOTAL SHEETS: 86	

NOT TO SCALE



TYPE C



SECTION A-A

PAVEMENT THICKNESS	BAR SIZE	
	DIAMETER	LENGTH
10" AND LESS	1 1/2"	18"
GREATER THAN 10"	1 3/4"	18"

DOWEL BASKET DETAILS

GENERAL NOTES:

THE DOWEL SUPPORTING UNITS SHALL BE FACTORY ASSEMBLED AND CAPABLE OF HOLDING THE DOWELS IN THEIR REQUIRED POSITIONS. IN THE COMPLETED JOINT INSTALLATION, NO DOWEL SHALL VARY FROM ITS REQUIRED POSITION MORE THAN 1 IN 36.

THE DOWEL BAR SPACING SHALL BE 12" CENTERS, BEGINNING 6" FROM THE OUTER EDGES OF THE PAVEMENT.

THE FREE END OF EACH EPOXY COATED DOWEL SHALL BE MARKED WITH A SPOT OF PAINT AT LEAST ONE INCH IN DIAMETER AND CONTRASTING IN COLOR WITH THE EPOXY COATING.

WIRES, BARS OR CLIPS SHALL BE USED AS NECESSARY TO STRENGTHEN THE ASSEMBLIES. TIE WIRES, BARS OR CLIPS USED TO STRENGTHEN DOWEL BASKET ASSEMBLIES FOR SHIPMENT AND INSTALLATION SHALL BE REMOVED OR CUT PRIOR TO PLACING CONCRETE.

STAKING PINS SHALL BE FABRICATED FROM 0 GA. WIRE MINIMUM WITH A SUITABLE HOOK. STAKING PINS SHALL HAVE A LENGTH OF 18" TYPE A AND TYPE B ASSEMBLIES AND 12" FOR TYPE C ASSEMBLY UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

MINOR VARIATIONS IN THE CONFIGURATION OF THE SUPPORT UNITS WILL BE ALLOWED.

