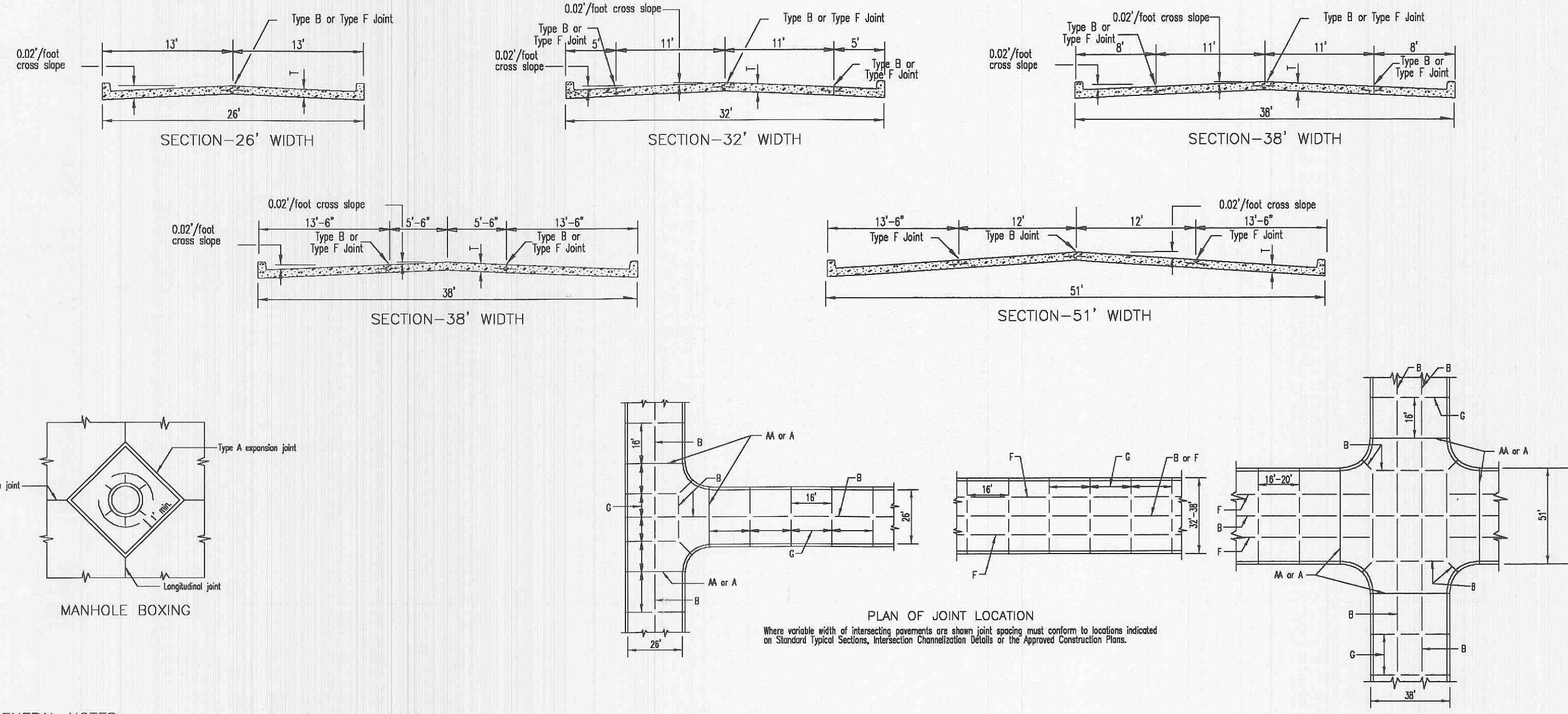


INTEGRAL VERTICAL CURB AND CONCRETE PAVEMENT TYPICAL SECTIONS AND DETAILS



GENERAL NOTES

- Do not scale drawing. Follow dimensions.
- All street inlets shall be separated from the pavement and curb by expansion joint material extending completely through curb and slab. Mortise castings within the pavement limits shall be boxed as shown.
- When a joint falls within 5ft. of, or contacts inlets, manholes, or other structures, shorten one or more points either side of opening to permit joint to fall on round structures and at or between corners of rectangular structures.
- Driveway configurations are shown in the "Entrance Construction Details".
- Construction joint and tie bars may be omitted when curb is poured monolithic with pavement.
- Minimum Thickness for Pavement is:

All Non-Residential Streets	7"
All Arterial Streets	8"

- Refer to Pavement Construction Details "Joints and Curb" for joint and bar requirements. Note that width and location of each poured portion of the pavement may change the type and location of joints required. See Std. Dwg. C502.03.
- Transverse or longitudinal construction joints in slip formed pavements may be made with a groover or tool, if such device has been approved in advance by the Department.
- The locations of the Type B and Type F longitudinal construction joints in the sections may be interchanged for different widths of construction if approved by the Department.
- All 1" smooth dowel bars 18"lg. @ 12" ctrs. shall be epoxy coated.

NOTE

Joint filler material shall meet ASTM designations as follows:
 Bituminous type ASTM D-994-03
 Non extruding & resilient bituminous type (fiber) ASTM D-1751-02T
 Non extruding & resilient non bituminous type (rubber) ASTM D-5249

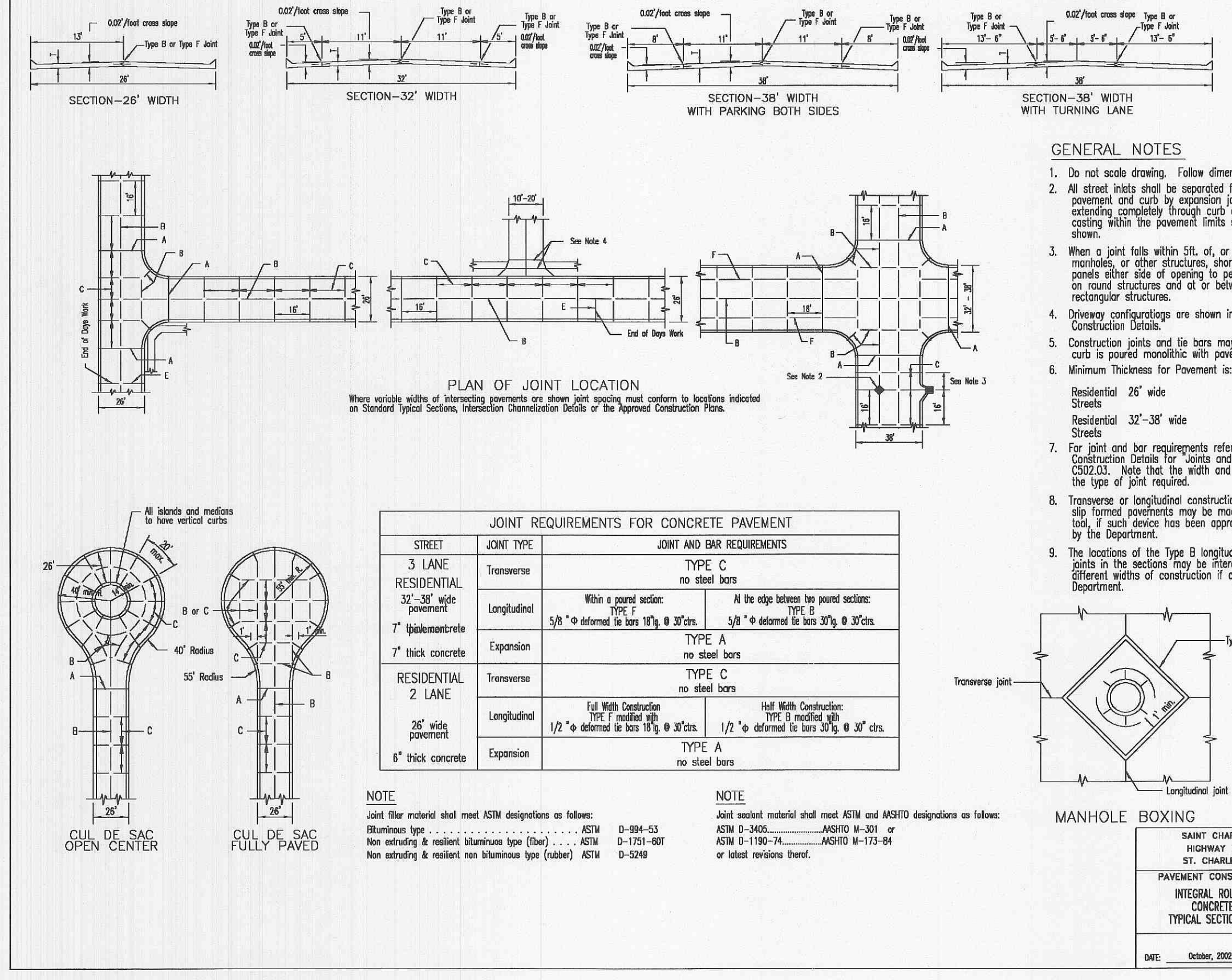
NOTE

Joint sealant material shall meet ASTM and AASHTO designations as follows:
 Bituminous type ASTM D-3465, AASHTO M-301
 Non extruding & resilient bituminous type (fiber) ASTM D-1190-74, AASHTO M-173-84
 Non extruding & resilient non bituminous type (rubber) ASTM D-5249 or latest revisions thereof.

STREET	JOINT TYPE	JOINT AND BAR REQUIREMENTS
All Arterial, Non-Residential	Transverse	TYPE C 1" smooth dowel bars 18"lg. @ 12" ctrs.
26'-51' wide pavement	Longitudinal	With a paved section: TYPE F 5/8" deformed tie bars 18"lg. @ 30" ctrs. At the edge between two paved sections: TYPE B 5/8" deformed tie bars 30"lg. @ 30" ctrs.
7'-8" thick concrete	Expansion	TYPE AA 3/4" smooth dowel bars 18"lg. @ 12" ctrs.

SAINT CHARLES COUNTY HIGHWAY DEPARTMENT
 ST. CHARLES, MISSOURI
 PAVEMENT CONSTRUCTION DETAILS
 INTEGRAL VERTICAL CURB AND CONCRETE PAVEMENT TYPICAL SECTIONS AND DETAILS
 DATE: October, 2002 DRAWING C502.01

INTEGRAL ROLLED CURB AND CONCRETE PAVEMENT TYPICAL SECTIONS AND DETAILS



STREET	JOINT TYPE	JOINT AND BAR REQUIREMENTS
3 LANE RESIDENTIAL 32'-38' wide pavement	Longitudinal	With a paved section: TYPE F 5/8" deformed tie bars 18"lg. @ 30" ctrs. At the edge between two paved sections: TYPE B 5/8" deformed tie bars 30"lg. @ 30" ctrs.
7' thick concrete	Expansion	TYPE A no steel bars
RESIDENTIAL 2 LANE 26' wide pavement	Transverse	TYPE C no steel bars
6" thick concrete	Expansion	TYPE A no steel bars

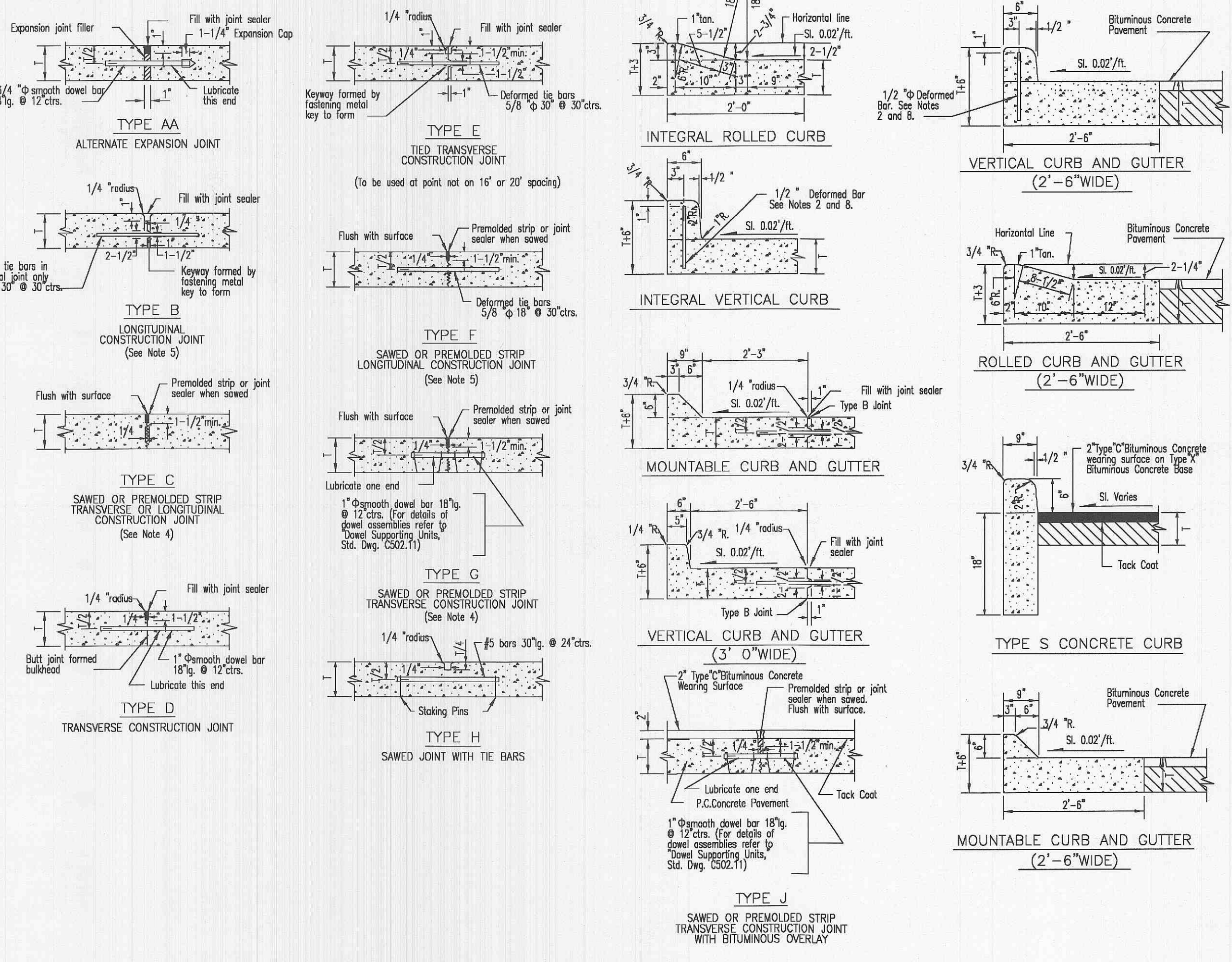
GENERAL NOTES

- Do not scale drawing. Follow dimensions.
- All street inlets shall be separated from the pavement and curb by expansion joint material extending completely through curb and slab. Mortise castings within the pavement limits shall be boxed as shown.
- When a joint falls within 5ft. of, or contacts inlets, manholes, or other structures, shorten one or more points either side of opening to permit joint to fall on round structures and at or between corners of rectangular structures.
- Driveway configurations are shown in the "Entrance Construction Details".
- Construction joints and tie bars may be omitted when curb is poured monolithic with pavement.
- Minimum Thickness for Pavement is:

Residential 26' wide Streets	6"
Residential 32'-38' wide Streets	7"
- For joint and bar requirements refer to the Pavement Construction Details for "Joints and Curb". See Std. Dwg. C502.03. Note that the width and location may change the type of joint required.
- Transverse or longitudinal construction joints in slip formed pavements may be made with a groover or tool, if such device has been approved in advance by the Department.
- The locations of the Type B longitudinal construction joints in the sections may be interchanged for the different widths of construction if approved by the Department.

MANHOLE BOXING

SAINT CHARLES COUNTY HIGHWAY DEPARTMENT
 ST. CHARLES, MISSOURI
 PAVEMENT CONSTRUCTION DETAILS
 INTEGRAL ROLLED CURB AND CONCRETE PAVEMENT TYPICAL SECTIONS AND DETAILS
 DATE: October, 2002 DRAWING C502.02



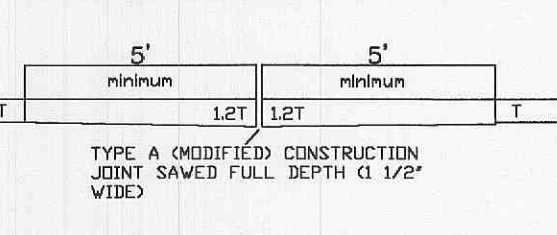
GENERAL NOTES

- Do not scale drawing. Follow dimensions.
- Construction joint and tie bars may be omitted when curb is poured integral with pavement.
- Minimum Thickness for Pavement is:

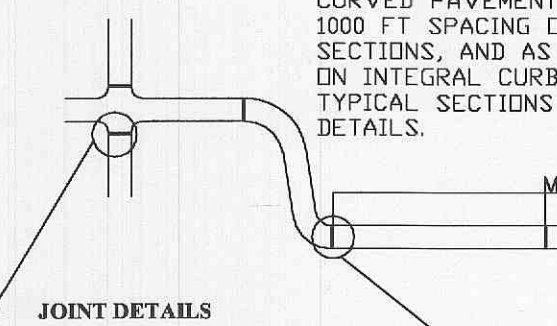
Bituminous Concrete Pavement Thickness = (1)	Concrete Pavement Thickness = (2)
All Residential Minor and Local Streets	6"
Residential Collector and Non-Residential Streets	7"
All Arterial Streets	8"
- 1" Transverse Joint is required for Arterial and Non-Residential streets. Use Type "C" Transverse Joint for all others.
- For Subdivision or Minor Streets having 6" concrete pavement, 1/2" deformed tie bars 30" long at 30" centers shall be used for Type B Longitudinal joints and 1/2" deformed tie bars 18" long at 30" centers shall be used for Type "F" Longitudinal joints.
- All deformed bars for joints and curbs shall be Galvalume Steel Bars conforming to ASTM A615-76, Grade 60.
- Length of the tie bars shall equal the thickness of pavement plus the height of curb less 2" tie bars shall be placed @ 24" centers.
- Transverse or longitudinal construction joints in slip formed pavements may be made with a groover or tool, if such device has been approved in advance by the Department.
- The free end of the dowel bar for a length of at least 11 inches shall be coated with an approved graphite grease.
- All dowel bars 18"lg. @ 12" ctrs. shall be epoxy coated.

SAINT CHARLES COUNTY HIGHWAY DEPARTMENT
 ST. CHARLES, MISSOURI
 PAVEMENT CONSTRUCTION DETAILS
 JOINTS AND CURBS
 DATE: September, 2002 DRAWING C502.03

TYPE A (MODIFIED) CONSTRUCTION JOINT



JOINT LOCATION



NOTES:

- THE NUMBER OF BACKER RODS REQUIRED WILL VARY WITH THE PAVEMENT THICKNESS, BACKER RODS AND FLEXIBLE FOAM FILLER MUST EXTEND THROUGH THE FULL DEPTH OF THE PAVEMENT.
- JOINT FILLER SHALL BE A FLEXIBLE, NON-ABSORBENT, NON-GASSING, NON-STAINING, NON-SHRINKING MATERIAL, EXTRUDED FROM A CLOSED-CELL POLYMER, ASTM D-5249.
- JOINT FILLER COMPOSED OF CELLULAR FIBERS, ASTM D-1751, MAY BE USED AT DRIVEWAY, SIDEWALK, AND SEWER INLET AND MANHOLE LOCATIONS.
- SEE "INTEGRAL CURB DETAIL 'B' AT DRIVEWAYS" FOR EXPANSION JOINT REQUIREMENTS AT DRIVEWAY CONNECTIONS TO THE STREET AND THE GARAGE.

SAINT CHARLES COUNTY HIGHWAY DEPARTMENT
 ST. CHARLES, MISSOURI
 PAVEMENT CONSTRUCTION DETAILS
 Type "A" Expansion Joint
 DATE: May, 2002 DRAWING C502.04



ISSUE	DATE	REMARKS
1	5/11/05	REVISED PER CITY COMMENTS
2	7/29/05	REVISED PER CITY COMMENTS
3		

PREPARED BY:
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 RESIDENTIAL - COMMERCIAL - MUNICIPAL

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 P.O. BOX 340
 ST. PETERS, MO 63376
 (636) 946-6871

PROJECT NO:
04021-00
 DESIGNED BY:
 DRAWN BY:
 CHECKED BY:
 SHEET NO: **11** OF **12**

PROJECT:
WABASH AVENUE ROAD IMPROVEMENTS
 CONSTRUCTION