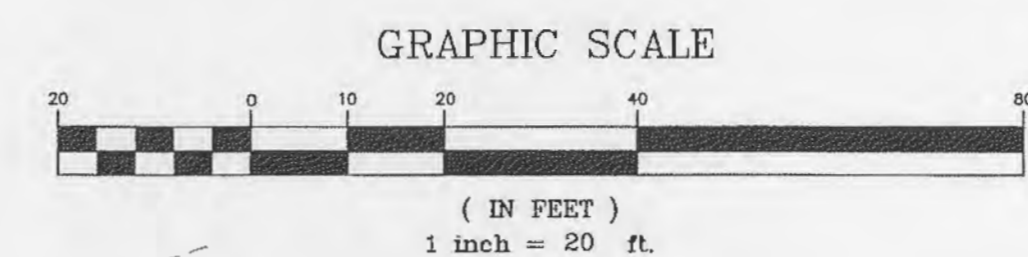
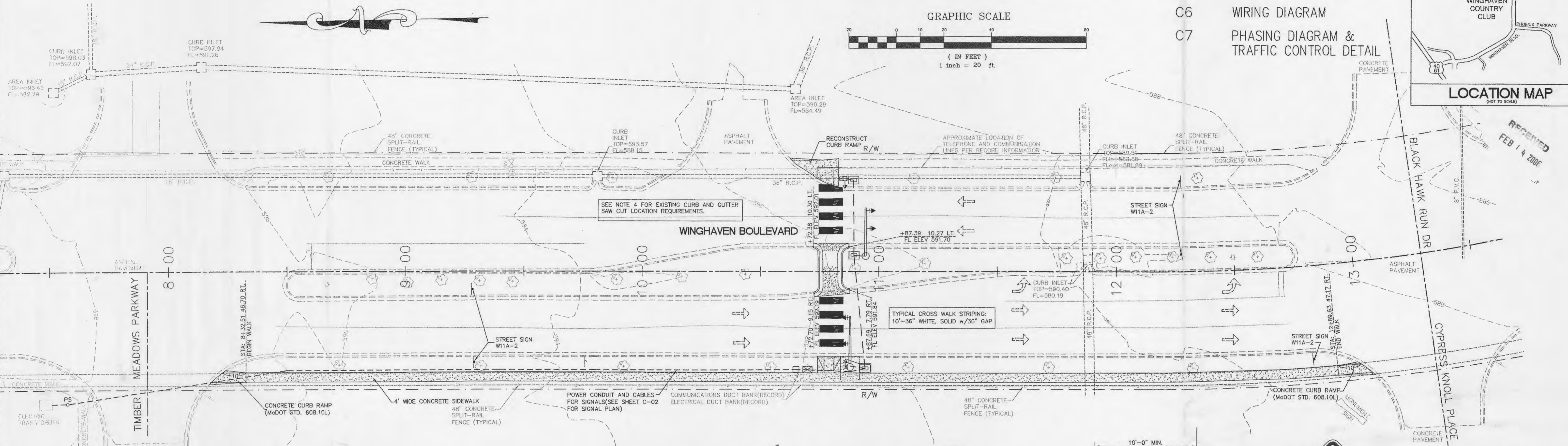


WINGHAVEN BLVD. SIDEWALK IMPROVEMENTS

(OMITTED FROM SET) C3-5 ~~SUMMARY OF QUANTITIES~~

INDEX

- C1 SIDEWALK IMPROVEMENTS
- C2 SIGNAL PLAN
- C3-5 ~~SUMMARY OF QUANTITIES~~
- C6 WIRING DIAGRAM
- C7 PHASING DIAGRAM & TRAFFIC CONTROL DETAIL



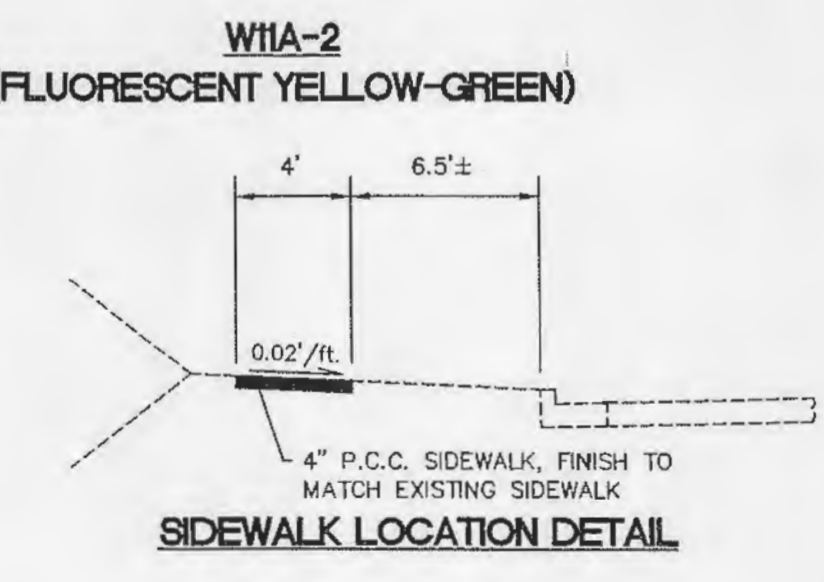
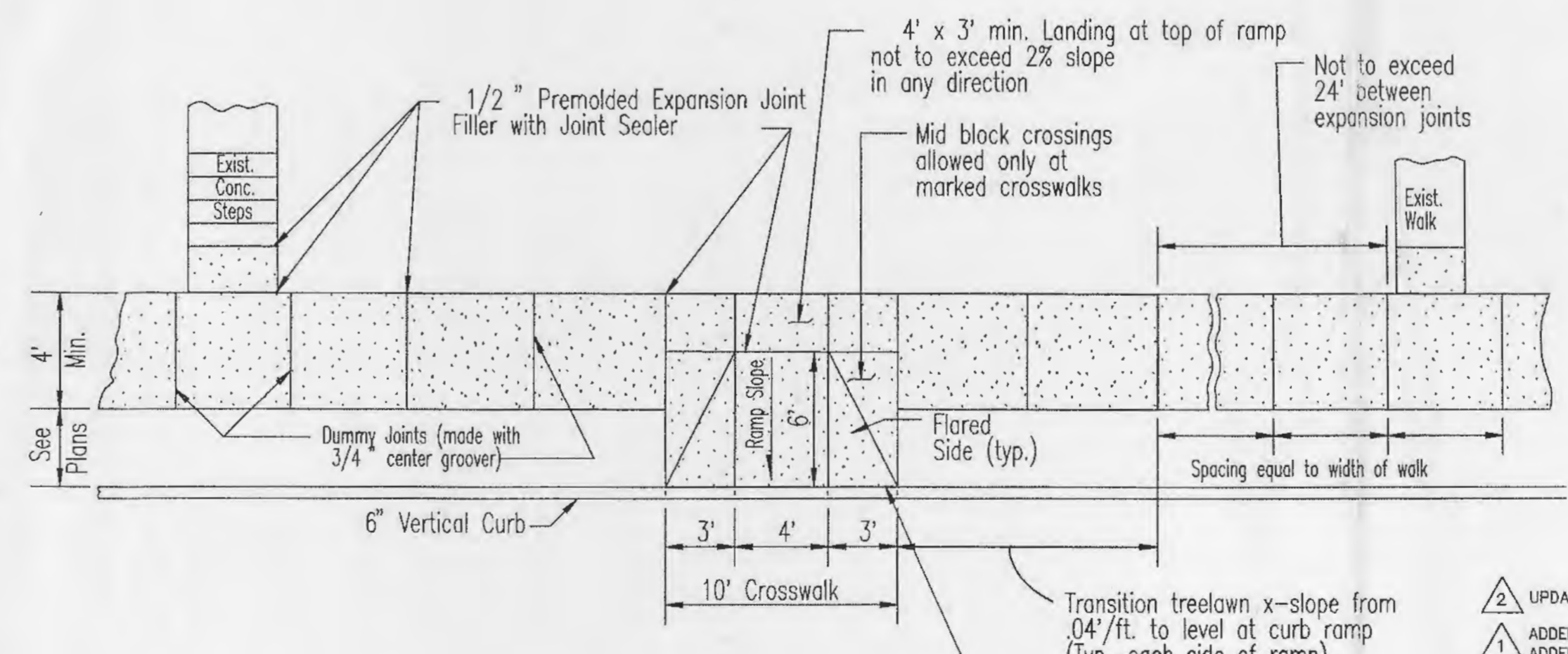
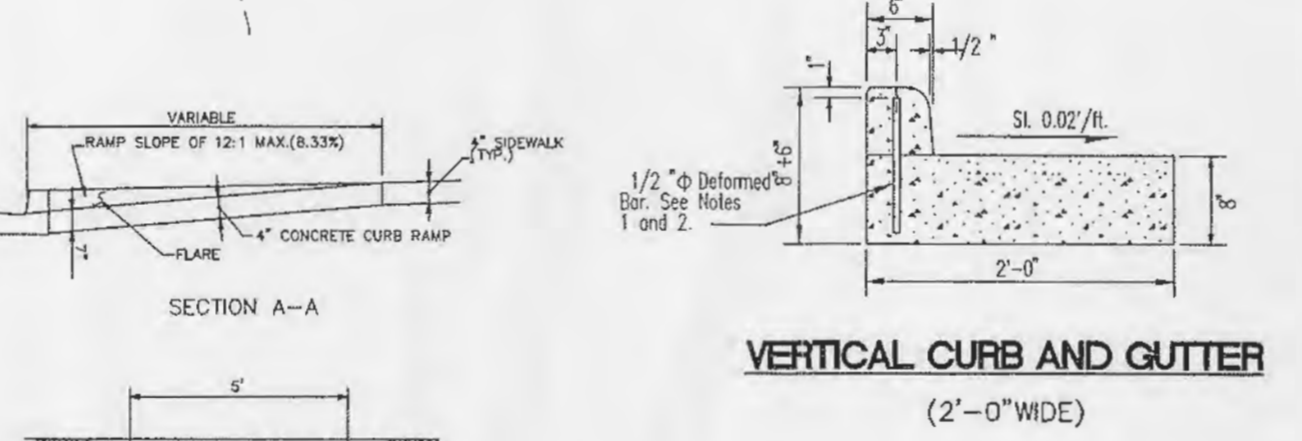
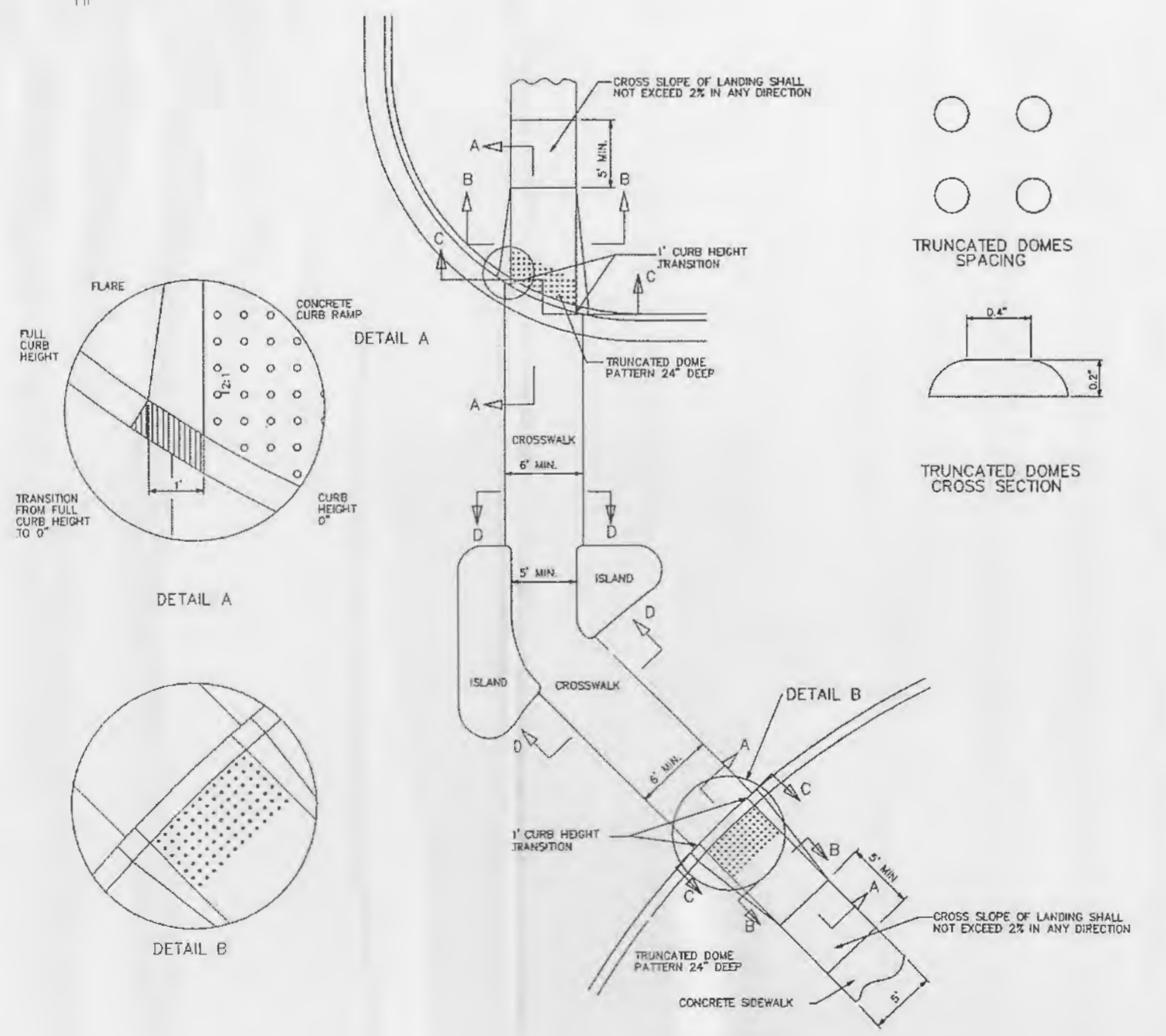
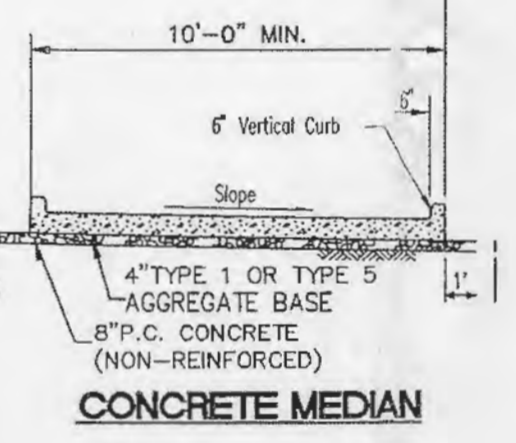
SEE NOTE 4 FOR EXISTING CURB AND GUTTER SAW CUT LOCATION REQUIREMENTS.

TYPICAL CROSS WALK STRIPING:
10'-36" WHITE, SOLID w/36" GAP

POWER CONDUIT AND CABLES FOR SIGNALS (SEE SHEET G-02 FOR SIGNAL PLAN)

COMMUNICATIONS DUCT BANK (RECORD) ELECTRICAL DUCT BANK (RECORD)

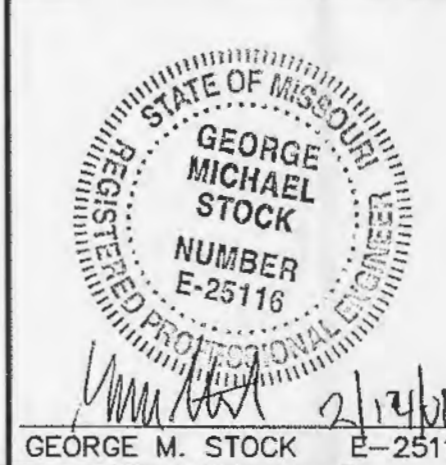
- NOTES**
- CONSTRUCTION JOINT AND TIE BARS MAY BE OMITTED WHEN CURB IS POURED INTEGRAL WITH PAVEMENT.
 - LENGTH OF THE TIE BARS SHALL EQUAL THE THICKNESS OF PAVEMENT PLUS THE HEIGHT OF CURB LESS 3". TIE BARS SHALL BE PLACED AT 24" CENTERS.
 - THE TYPE OF JOINTING TO BE USED IN CONSTRUCTION OF THE PAVEMENT SHALL BE DECIDED IN THE FIELD BY THE ENGINEER.
 - ALL EXISTING CURB AND GUTTER SHALL BE SAW CUT AT NEAREST JOINT IF THE IMPROVEMENTS END WITHIN FIVE (5) FEET FROM THE JOINT.



UTILITY NOTE:
UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS, RECORDS AND INFORMATION, AND THEREFORE DO NOT NECESSARILY REFLECT THE ACTUAL EXISTENCE, NON-EXISTENCE, SIZE, TYPE, NUMBER, OR LOCATION OF THESE FACILITIES, STRUCTURES AND UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE ACTUAL LOCATION OF ALL UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES, EITHER SHOWN OR NOT SHOWN ON THESE PLANS. THE UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES SHALL BE LOCATED IN THE FIELD PRIOR TO ANY GRADING, EXCAVATION OR CONSTRUCTION OF IMPROVEMENTS. THESE PROVISIONS SHALL IN NO WAY ABSOLVE ANY PARTY FROM COMPLYING WITH THE UNDERGROUND FACILITY SAFETY AND DAMAGE PREVENTION ACT, CHAPTER 319 RSMo.

BENCHMARK
SC-13, LOCATED 2.70 MILES NORTH/NORTHWEST OF THE INTERSECTION OF HIGHWAY 94 AND HIGHWAY D ON THE NORTH SIDE OF WESTBOUND LANES OF HIGHWAY 40 NEAR A FIELD ENTRANCE GOING NORTH FOR THE WESTBOUND LANES OF HIGHWAY 40. IT IS 29.53' NORTHEAST OF A P.K. NAIL IN THE CENTER LINE OF WESTBOUND HIGHWAY 40, 39.2' SOUTHWEST OF THE CENTER OF THE TOP OF THE NORTHWEST END OF A CORRUGATED METAL PIPE. ELEV.=499.34'

GENERAL NOTES:
12:1 MAXIMUM SLOPE ON ALL CONCRETE CURB RAMPS, PROVIDE A LANDING THE SAME WIDTH AS CURB RAMP AND 5' LONG ADJACENT TO CURB RAMP WITH MAXIMUM 2% SLOPE IN ANY DIRECTION.
TRIED CONCRETE (AS PER STANDARD SPECIFICATION 1056) SHALL BE USED ON CURB RAMP ONLY.



WINGHAVEN BLVD. - SIDEWALK IMPROVEMENTS
PLAN

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